



## **PLANNING COMMISSION AGENDA**

June 9, 2025 – 6:30 PM

Stanwood Fire Station (8117 267<sup>th</sup> Pl NW)

1. Call to Order
2. Roll Call
3. Public Requests and Comments
4. Approval of Minutes
  - Approval of the May 12, 2025, Planning Commission Meeting Minutes
5. Old Business
  - Missing Middle Housing and Land Development Code Amendments Discussion
6. New Business
  - Multimodal Level of Service
7. Miscellaneous Business
8. Recent Council Action on Commission Items
  - Critical Areas Code Amendment
9. Upcoming Items
10. Adjourn

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### **Zoom Meeting Information**

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Please click the link below to join the webinar:

<https://us02web.zoom.us/j/82891360016>

Passcode: 502157

Telephone: 253-215-8782

Webinar ID: 830 9911 3579

## **May 12, 2025 Planning Commission Meeting Minutes**



Planning Commission  
Meeting Minutes  
Monday, May 12, 2025 – 6:30 pm

**Call to Order:** 6:30 p.m.

**Roll Call**

**Commissioners Present:**

Melissa Toner, Commissioner  
Patrick Hosterman, Commission Chair  
Cody Davis, Commission Vice-Chair  
Gabrielle Braley, Commissioner  
Jeff Wheatley, Commissioner

**Staff Present:**

Patricia Love, Community Development Director  
Tansy Schroeder, Senior Planner  
Audrey Rotrock, Associate Planner

**Absent:** Eric Warnat, Commissioner

**Also known to be present:** Russell Joe (MBAKS), Richard Craig

**Public Requests and Comments:** Russell Joe requested the Commissioners to be flexible while reviewing Missing Middle Housing. Mr. Joe also asked the Commissioners to closely examine the frontage improvement requirements. Frontage improvements could potentially be cost prohibitive to a project.

**Approval of Minutes:**

The minutes from the April 14, 2025, Planning Commission meeting were unanimously approved with one grammar correction.

**New Business:**

- None

**Old Business:**

**Missing Middle Housing Discussion**

Missing middle housing refers to a range of multi-unit or clustered housing types—such as duplexes, triplexes, townhomes, and cottage courts—that are compatible in scale with single-family neighborhoods. The purpose of missing middle housing is to fill the gap between traditional single-family homes and large apartment complexes, providing more diverse, affordable, and flexible housing options for a variety of household types and income levels. This type of housing supports walkable communities, efficient land use, and helps cities meet growing demand for housing without drastically altering neighborhood character. Staff presented multiple examples of what missing middle housing might look like on a real piece of property in Stanwood.

**Commissioner Questions & Comments**

- Could certain requirements, such as open space for example, be waived? The City could look in to developing a Fee in Lieu to waive requirements, but that could drive the price of the housing up. Staff will bring more information to the next meeting with examples of some other trade-offs.
- Could green space be waived if there is a park or other open space nearby? The amount of green space required is dictated by the number of units in the development. This requirement comes from the Stanwood Municipal Code (SMC), not the State. The City



## Planning Commission Meeting Minutes Monday, May 12, 2025 – 6:30 pm

can consider revising its open space requirements, including possible amendments to include open space connectivity with that portion of the Municipal Code Update project.

- The 22 lots in the example is calculated based on the gross acreage of the property.
- Would a developer have a say in how the units can be configured on a parcel? Yes, the examples shown are a few ideas of potential configurations.
- What would entice a developer to build a cluster type configuration of units? With the clustering of units, there will be less paving, less utility and less infrastructure costs.
- In the example where half the property is open space, what would be allowed there? The open space could be left as it is or walking trails could be added, for example.
- The cluster example looks like they would become rentals. Staff commented that they could also be sold as condos.
- The cluster example would change the look of traditional Stanwood.
- Could the large open space be split up? Yes, but that would increase the infrastructure cost again.
- The cluster concept is similar to places in Europe and might be the direction that housing is going. Large lots aren't sustainable and are not affordable for the next generation. Having the large open space is good when the units have small yards.
- Staff will bring back more examples with different configurations of open space.
- The City provides the flexibility by allowing different mixes of housing and the market decides how and what to build.
- There is a large population of aging residents in Stanwood that are looking to downsize to smaller one-story homes.
- Maximize the amount of lots on infill properties. Incentivize developers to build missing middle type housing.

### **Miscellaneous Business:**

#### **Keller Annexation**

Tyson O'Neil, representing KO Group LLC and KO2 LLC, has formally submitted a petition to annex approximately 26.75 acres of land located within the City of Stanwood's eastern Urban Growth Area (UGA). This marks the initial step in the annexation process, commonly referred to as the "10% petition."

The subject property currently carries a pre-zoning designation of *Traditional Neighborhood – Mixed Use (TN-MU)*, which requires a mix of residential and commercial uses. The owner's primary interest lies in developing senior and assisted living facilities, along with a mix of commercial and light industrial incubator spaces intended to support small and startup businesses.

To move forward with a project that focuses exclusively on senior living and incubator-style commercial/light industrial development, the applicant would need to request an amendment to the City's Comprehensive Plan and pursue a rezone as part of the annexation process. A more appropriate zoning approach would involve a combination of *Multifamily Residential (MR)* and *Planned Industrial (PI)* designations.

Any future rezones or Comprehensive Plan amendments associated with the annexation will be subject to review by the Planning Commission.



Planning Commission  
Meeting Minutes  
Monday, May 12, 2025 – 6:30 pm

Commissioner Questions & Comments

- No questions or comments.

**Recent Council Action on Commission Items:**

- Critical Areas Code Amendment

**Upcoming Items:**

- Multimodal Level of Service

**Other:**

- Richard Craig attended the meeting. Mayor Roberts will be nominating Richard as Planning Commissioner at the next City Council meeting.
- There are a couple of Open House meetings coming up about small scale commercial projects on infill lots.

**Adjourn:** 8:13 pm

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## **June 9, 2025 Planning Commission Meeting Staff Report**



CITY OF STANWOOD  
PLANNING COMMISSION

AGENDA STAFF REPORT

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**MEETING DATES:** June 9, 2025  
**SUBJECT:** Agenda Items  
**CONTACT PERSON:** Patricia Love, Community Development Director

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### *Land Division Code / Missing Middle Housing:*

Over the past several months, the City has been actively developing proposed amendments to its subdivision and permitted use codes. These updates aim to better accommodate both the protection of critical areas and the integration of "missing middle" housing types—such as duplexes, triplexes, townhomes, and cottage housing. The overarching goal is to promote development flexibility that thoughtfully balances environmental stewardship with the need to provide housing options for a range of income levels.

In May, the Planning Commission was given the first draft of the Missing Middle Housing and Land Division Ordinances for review. The updates in this ordinance include two groups: 1) zoning code revisions to implement policies of the 2024 Comprehensive Plan related to missing middle housing and limited commercial infill; and 2) an overhaul of the existing Title 16 Subdivisions into a new chapter of the Unified Development Code Title 18 Division 4: Land Divisions and Adjustments.

Below is a more detailed summary of the proposed changes.

- 1) Comprehensive Plan Implementation
  - a. Revise code to implement new state requirements regarding the citing and regulation of:
    - i. Adding housing units in existing non-residential buildings;
    - ii. Accessory dwelling units
    - iii. Emergency shelters, permanent supportive housing, and shelters
    - iv. Co-living housing
    - v. Revised parking requirements related to missing middle housing types
  - b. Inclusion of "missing middle" housing types, such as duplexes, multiplexes, townhouses, and cottages in residential zones.
  - c. Add new provisions to allow limited commercial infill, "corner store concept", development on certain Uptown sites.

- d. Revise definitions in Title 17.20, as needed, to support new proposed code changes.
  - e. Revise permitting procedures in Title 18 Division 2 Applications to support new proposed code sections.
- 2) Title 18 Division 4 Land Divisions and Adjustments
- a. Revise and add new code sections to implement new state laws relating to:
    - i. Unit lot subdivisions
    - ii. Zero lot line developments
    - iii. Split lot subdivisions
  - b. Add new code sections to address city development priorities, including:
    - i. Lot size averaging
    - ii. Density transfer
    - iii. Model home development
  - c. Update and move lot size and density standards from Title 17 Zoning to new section in Land Divisions and Adjustments chapter.
  - d. Update definitions in Title 18 Division 1, as needed to support new proposed code changes.
  - e. Add graphics and charts to illustrate code concepts.
  - f. Overall, reorganize and streamline existing Title 16 Subdivisions into a new chapter that is easier to use, avoids confusing duplicate language, and conforms to state law.

***Revised Lot Development Standards:***

At the May 8 Worksession, the City Council reviewed a presentation analyzing how the City’s existing development regulations may unintentionally restrict the ability of properties to achieve the residential densities envisioned by the Comprehensive Plan and permitted under current zoning designations. This same presentation was subsequently shared with the Planning Commission during their May 12 meeting.

The presentation outlined several specific regulatory barriers that contribute to this discrepancy. These include restrictive minimum lot sizes, setback requirements, road layout standards, and open space provisions—all of which can diminish the actual number of housing units developed on a site, even when higher densities are allowed by zoning.

Both the Council and the Planning Commission expressed general support for expanding access to “missing middle” housing types, such as duplexes, triplexes, and townhomes, which are critical for addressing housing diversity and affordability. However, some members raised concerns regarding proposed reductions in lot sizes and the implementation of clustering options—particularly the concept requiring 50% of the development site to be preserved as open space.

In response to the feedback received, City staff have developed revised proposals for further discussion and evaluation:

Revised Minimum Lot Size Standards: To address concerns related to neighborhood compatibility and community character, staff proposes to modify the originally suggested minimum lot size of 3,000 square feet.

- Minimum Lot Size: Proposal to maintain flexibility through lot size averaging provisions while responding to concerns about density impacts and scale.
  - A minimum lot size of 5,000 square feet for the SR 12.4, SR 9.6, and SR 7.0 zones.
  - A minimum lot size of 3,000 square feet for the SR 5.0 and Multi-Family (MF) zones.
  
- 1. Revised Clustering Concept: Given the apprehension surrounding the initial clustering concept—which proposed preserving 50% of the development site as open space—staff have drafted a revised clustering strategy. This updated model:
  - Applies the 5,000 square foot minimum lot size.
  - Focuses on creating smaller, more manageable clusters of homes.
  - Encourages the integration of missing middle housing types within each cluster.
  - Incorporates open space buffers between clusters, rather than a fixed percentage of open space across the entire site.

These revised proposals are intended to balance the City’s goals for increased housing diversity with the need to maintain compatibility with existing neighborhood patterns.



Implementing the proposed development concept involves a series of trade-offs that reflect a balance between community character, housing diversity, and land use efficiency. Key considerations include the following:

### 1. Greater Flexibility in Site Design through Reduced Lot Sizes

Reducing the minimum lot size—particularly in designated higher-density zones—provides developers with increased flexibility in how a site is configured. This can lead to more efficient site layouts, integration of different housing types, and the ability to address site-specific topographic or critical area constraints. Smaller lots also create opportunities to accommodate a broader variety of missing middle housing, such as duplexes, townhomes, and cottage courts, within compact, walkable neighborhoods.

### 2. Open Space as Buffers Instead of Large Centralized Play Areas

The revised clustering approach proposes using open space as buffers between smaller clusters of homes rather than requiring large, centralized parks or play fields. This design strategy offers several benefits:

- Enhanced privacy for individual housing clusters by separating them with green space.
- Noise and visual screening between different housing types or adjacent developments.
- Opportunities for low-impact landscaping, habitat preservation, and stormwater infiltration. While this may result in fewer large recreational spaces, the smaller, strategically placed open areas can still contribute meaningfully to the neighborhood's aesthetics and livability.

### 3. Continued Support for Housing Type Diversity

The proposed concept preserves the City's objective to support a diverse housing inventory. By allowing smaller lots and greater clustering flexibility, it becomes easier to integrate a mix of housing types within the same development. This diversity helps meet the needs of various household sizes, ages, and income levels, while also advancing goals related to housing affordability and choice.

### 4. Improved Alignment with Gross Density Goals

The revised concept brings development outcomes closer to the gross density figures envisioned in the City's Comprehensive Plan and zoning ordinances.

- Gross density refers to the total number of units permitted on a property based on its size and zoning designation, before deductions for infrastructure and environmental constraints.
- Net density, by contrast, reflects the number of units actually achieved after accounting for land set aside for roads, stormwater facilities, parks, critical areas, and other non-developable features.

### **Street Standards:**

Amending street standards is another strategy that the city can use to help meet density goals and accommodate missing middle housing types. Traditional street design requirements—such as wide public rights-of-way and limits on the number of units allowed on private roads—were often developed with large-scale subdivisions in mind and may not be well-suited for today's infill and compact housing developments.

Revisiting these standards can offer greater site design flexibility and make it easier to optimize the use of available land. Staff is proposing the following options for consideration:

### 1. Amend Stanwood's Private Road Standards

One strategy to facilitate infill development is to increase the number of lots or dwelling units permitted on a private road. This option can be especially advantageous for smaller or irregularly shaped parcels, where constructing a full public roadway may be impractical, cost-prohibitive, or result in an inefficient use of land. Allowing additional units on a private access road can unlock development potential without compromising safety or accessibility.

The number of lots permitted on a private road is determined by individual jurisdictions, and practices vary widely. A brief survey of comparable cities and counties reveals that jurisdictional limits range from as few as 4 lots to as many as 50. Currently, Snohomish County currently allows up to 9 lots to be served by a private road.

City staff is recommending an increase in the current limit from 4 to 9 lots on a private road. This recommendation is consistent with the maximum number of lots allowed under a short plat, thereby simplifying the infill development process and reducing regulatory barriers for small- to mid-sized projects. Importantly, all developments would still be required to comply with essential fire and life safety standards, including adequate road width, emergency vehicle access, and turnarounds, to ensure public safety is not compromised.

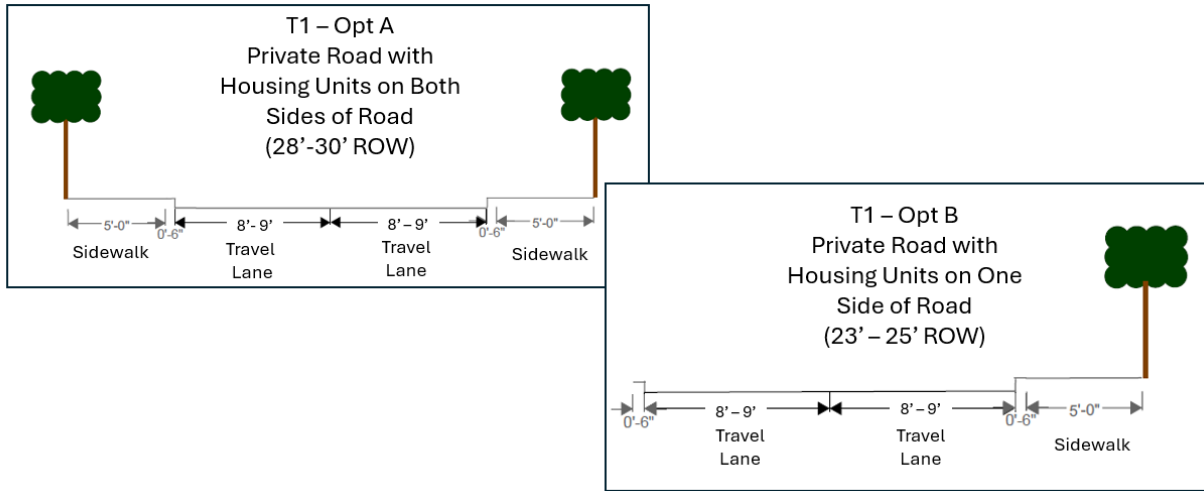
To support this proposed policy change, updated roadway cross-sections are currently being developed for review and discussion. These revised designs aim to preserve safety and accessibility while promoting more efficient land use and supporting increased housing production.

#### Tier 1: Private Roads

- Designed to serve between 2 and 9 residential units
- Feature two travel lanes
- Include sidewalks on one side of the street, or on both sides if homes are located on both sides of the roadway

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## Tier 1: Private Road 2-9 Units



### 2. Provide Flexibility in Public Roadway Standards

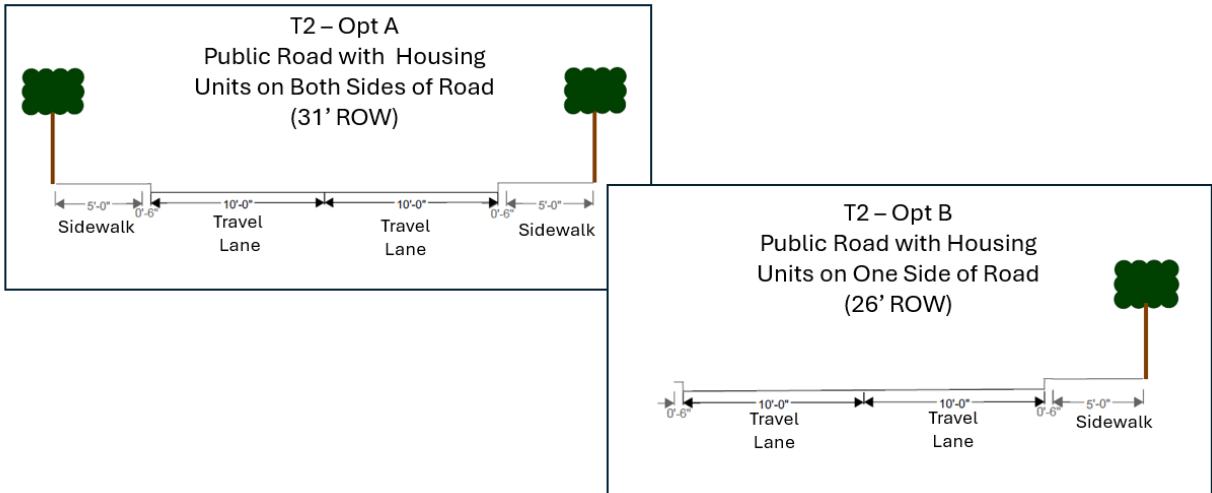
Another option is to reduce minimum public roadway widths for medium-sized infill developments, where traditional road dimensions may be excessive. Narrower streets can not only preserve land for housing but also help to calm traffic, improve pedestrian safety, and contribute to neighborhood scale. These modified street standards can also reduce infrastructure costs, making smaller projects more financially viable and attractive to developers.

Proposed updates to the public road standards are also underway, with two new tiers under consideration to better accommodate varying scales of development.

- **Tier 2 – Reduced Roadway Standards:** Intended for small infill developments ranging from 10 to 20 residential units, this tier features a narrower roadway design to support efficient land use while maintaining safety and accessibility.
- **Tier 3 – Full Street Standards:** Applied to larger developments with 21 or more units, this tier retains the full roadway specifications consistent with traditional urban street design to support higher traffic volumes and infrastructure needs.

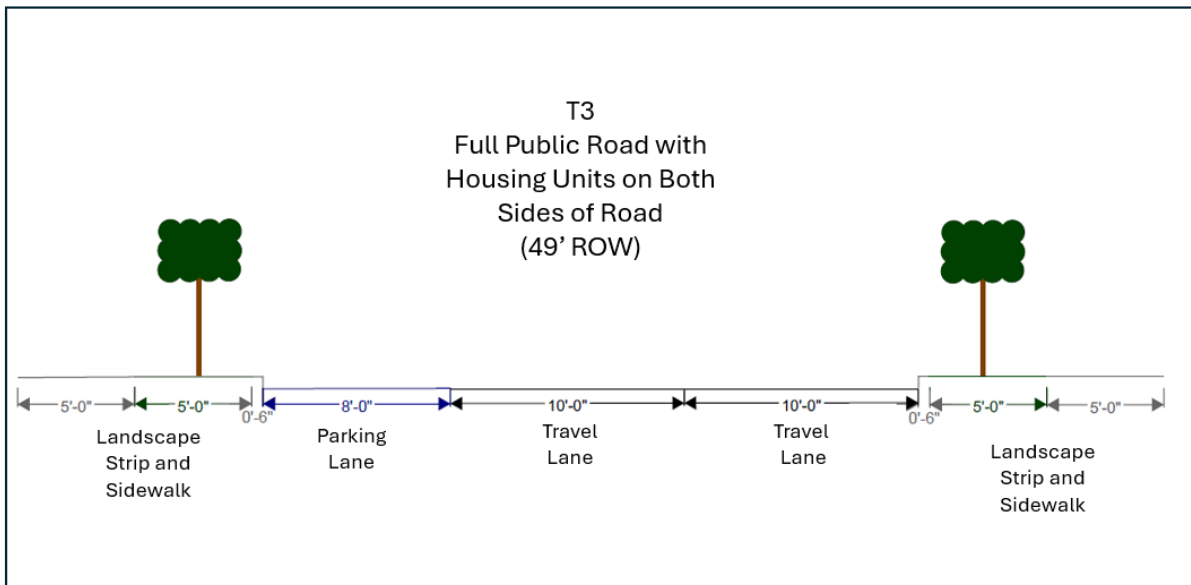
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### Tier 2: Reduced Public Road 10-20 Units



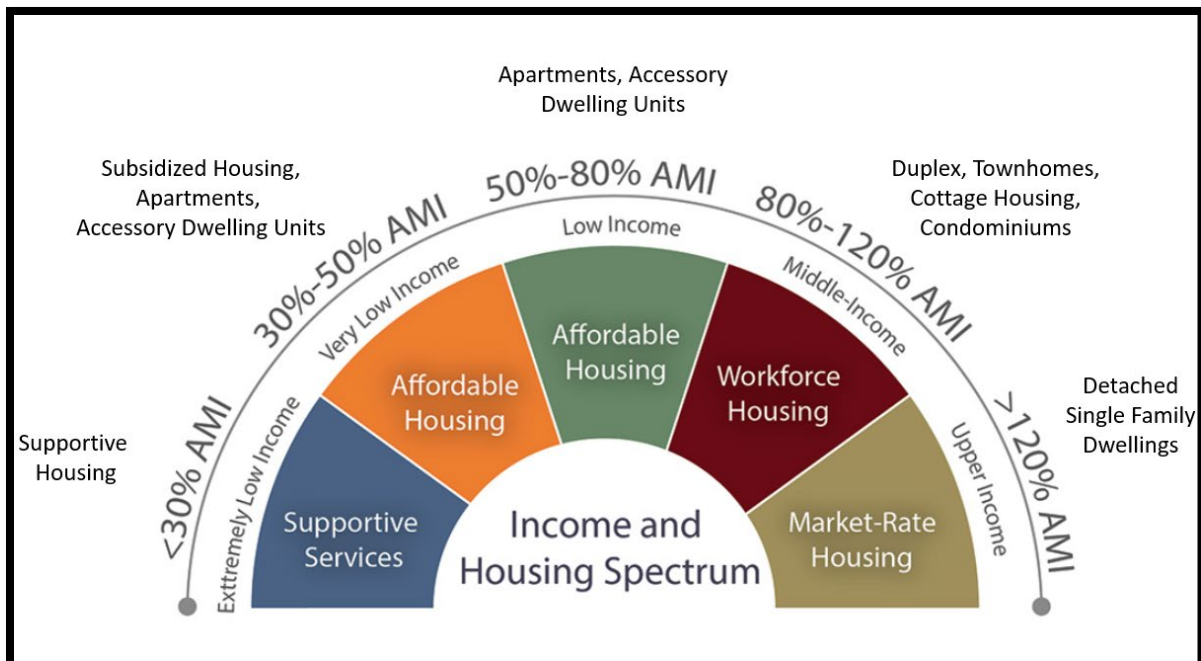
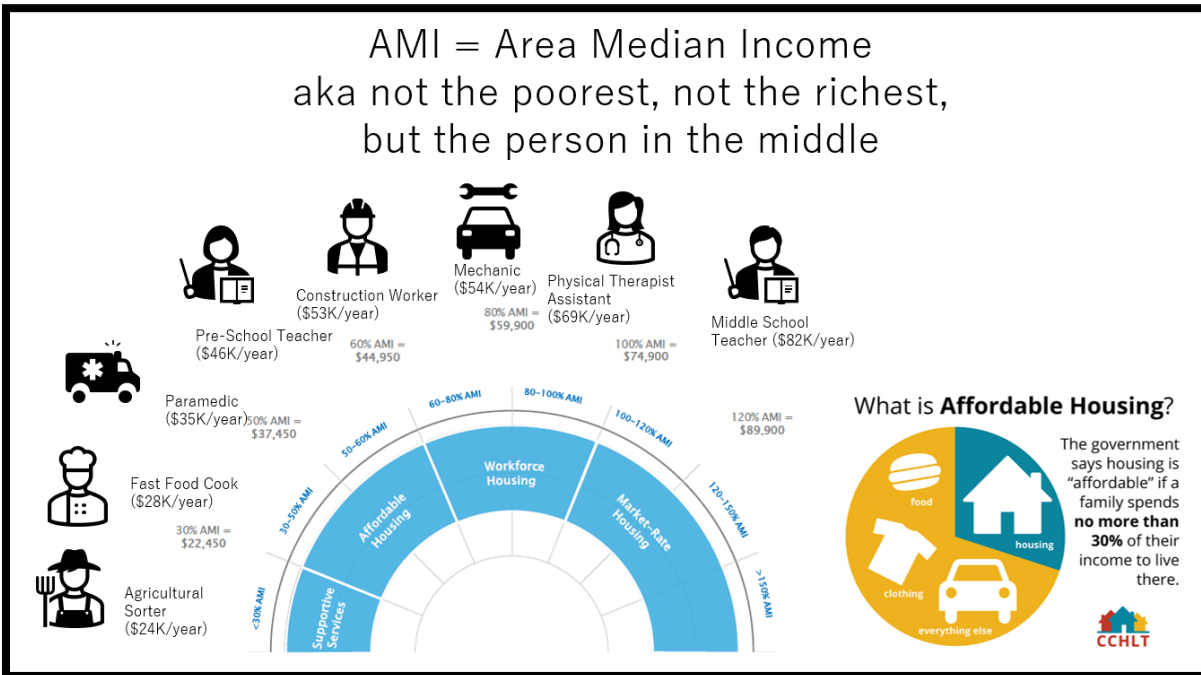
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### Tier 3: Full Public Road 21 + Units (Existing Street Standard)



Ultimately, these types of amendments support the creation of walkable, diverse neighborhoods that incorporate a mix of housing types and price points. When thoughtfully implemented, they provide a critical balance between achieving planned

density targets, maintaining public safety and accessibility, and promoting smart growth principles in both new and existing residential areas. As requested by the Planning Commission, below are some graphics reminding the Commission of housing affordability to income levels.





Source: City of Everett Housing Webpage

***Uptown Commercial – Corner Store – Concept:***

In 2003, the City adopted the Traditional Neighborhood zoning designation, which included provisions to reintroduce the “corner store” concept into the Uptown neighborhood. However, in the more than 20 years since its adoption, development within this zone has been exclusively residential. Despite the original intent, residential builders have not incorporated corner store spaces into their projects.

As part of the 2024–2044 Comprehensive Plan update, the City has adopted new housing policies that call for a reevaluation of Uptown commercial uses—specifically the reintroduction of corner stores. These small-scale commercial spaces play a key role in creating vibrant, walkable neighborhoods by providing residents with convenient access to everyday necessities and services, often within walking distance of home. When integrated thoughtfully alongside missing middle housing types, corner stores contribute to mixed-use environments that blend increased residential density with neighborhood-scale commercial activity, fostering both community interaction and local economic vitality.

To solicit public input on this concept, the City hosted an open house—both online and in person—on May 29. The event was promoted through the City’s social media channels and generated over 400 public comments. Attached is the presentation providing a summary of the proposal and feedback.

## *Multimodal Level of Service Standard:*

The Washington State Growth Management Act (GMA) mandates that cities develop and adopt Comprehensive Plans to effectively guide their long-term growth and development. A crucial part of Comprehensive Plans is the Transportation Element, which focuses on creating and maintaining efficient, sustainable, and multimodal transportation systems.

GMA requires cities to adopt multimodal level of service (MMLOS) standards. Specifically:

“Multimodal level of service standards for all locally owned arterials, locally and regionally operated transit routes that serve urban growth areas, state-owned or operated transit routes that serve urban areas if the department of transportation has prepared such standards, and active transportation facilities to serve as a gauge to judge performance of the system and success in helping to achieve the goals of this chapter consistent with environmental justice.”

Stanwood has adopted Level of Service Standards for vehicles based on roadway type and intersection controls. To be compliant with state law, the City is initiating a project to evaluate appropriate MMLOS for the City and adopt implementing policies and regulations. The goal of the project is to expand the current vehicle-centric standards to a more inclusive and sustainable approach to mobility that benefits all transportation modes and users.

Transpogroup, the City’s on-call transportation planning consultant, have prepared their initial assessment and recommendations on how best to proceed with adopting a MMLOS standard for the City. They have been tasked with:

Preparing a multimodal concurrency and LOS program(s) that meets the requirements of the Growth Management Act and Puget Sound Regional Council (PSRC) VISION 2050. MMLOS standards should align with community goals, available and anticipated funding, and city growth targets. It is anticipated that work will include:

Attached is their draft summary report with their suggested approach. The Council Public Works and Community Development Committees are reviewing the approach at their June meetings. Staff will present their recommendations at the meeting.

## **Small Scale Commercial Uses in Uptown Neighborhood “Corner Store” Presentation**



CITY OF STANWOOD

# STANWOOD MUNICIPAL CODE UPDATE

Small Scale Commercial Uses In Uptown Neighborhood  
“Corner Store”

*Community Open House*

*May 29, 2025*



CITY OF STANWOOD

MUNICIPAL CODE UPDATE

# Neighborhood Small-Scale Commercial

## Proposal: What

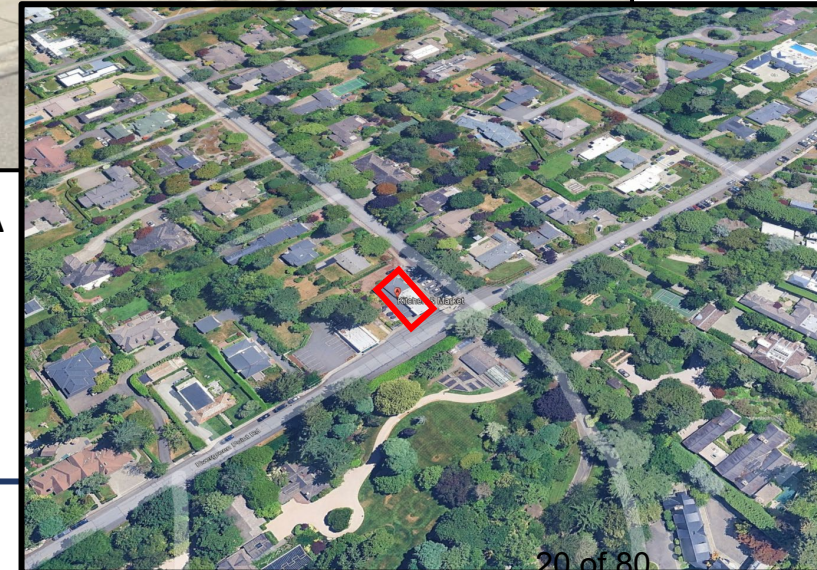
### What is a Corner Store?

A small retail shop located within a neighborhood or community, often at a street corner—hence the name.

These stores are known for their accessibility, convenience, and central role in serving daily needs.



Medina, WA

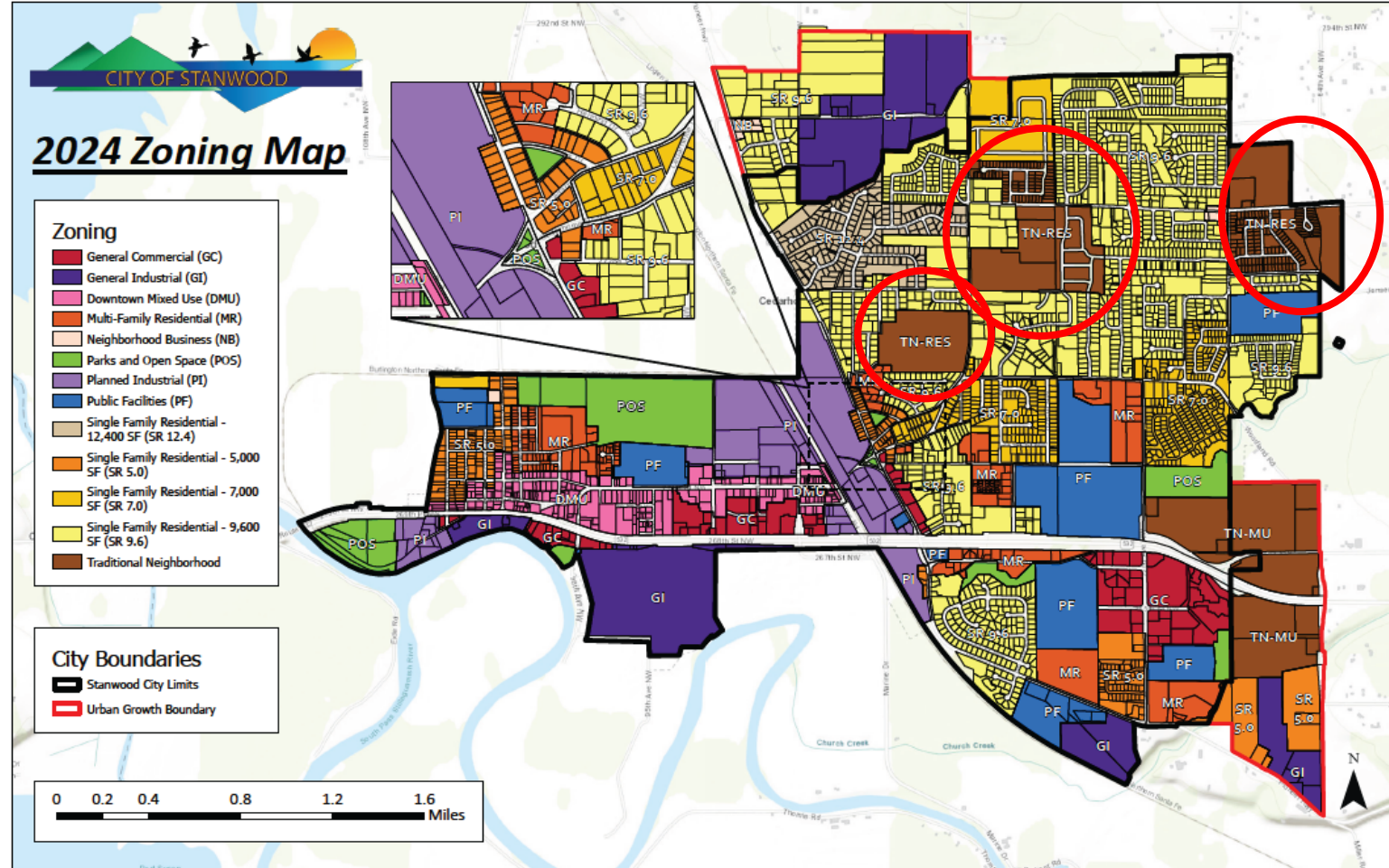


# Neighborhood Small-Scale Commercial



## Proposal: Why

- Started With Traditional Neighborhood Zoning in 2003
  - Encourage Walkable Neighborhoods
  - Encourage Small Scale Mixed-Use Neighborhoods
  - Adopting Traditional Town Design Principles
- What Happened:
  - Only Residential Development Occurred
  - No Commercial Uses



Brown = Traditional Neighborhood Zoning



# Neighborhood Small-Scale Commercial

## Proposal: Why

### Challenges:

- 20 Years Since Adoption of TN Zoning
- Only Residential Uses Built
- Residential Builders Don't Build Commercial Spaces
- TN Tries to Force Commercial Uses in Developments



Volunteer Park Café, Seattle

# Neighborhood Small-Scale Commercial

## Proposal: Comprehensive Plan Policy

### *2024-2044 Comprehensive Plan:*

- Revisit the Corner Store Concept
- Provide Location Flexibility vs. TN Zone Only
- Only Uses That Support the Neighborhood
- Small in Scale
- Complementary to Residential Building Sizes



LUP 9.8: Consider an amendment to the Municipal Code allowing small scale independently owned commercial uses along arterials in the uptown residential area to provide services that support the immediate needs of the neighborhood, support neighborhood livability and pedestrian-oriented needs such as commercial day cares, coffee shops, and personal services. Uses and buildings should be compatible with the associated neighborhood scale by considering parking, landscaping, building height, design, pedestrian access, and the appropriate permitting process. Strip malls, commercial chain businesses, or franchises do not meet the intent of this policy.

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# Neighborhood Small-Scale Commercial

## Proposal: Where

### ■ Direct Frontage On:

284<sup>th</sup> Street NW

272<sup>nd</sup> Street NW

68<sup>th</sup> Avenue NW

80<sup>th</sup> Avenue NW

276<sup>th</sup> Street NW

268<sup>th</sup> Street

72<sup>nd</sup> Avenue NW

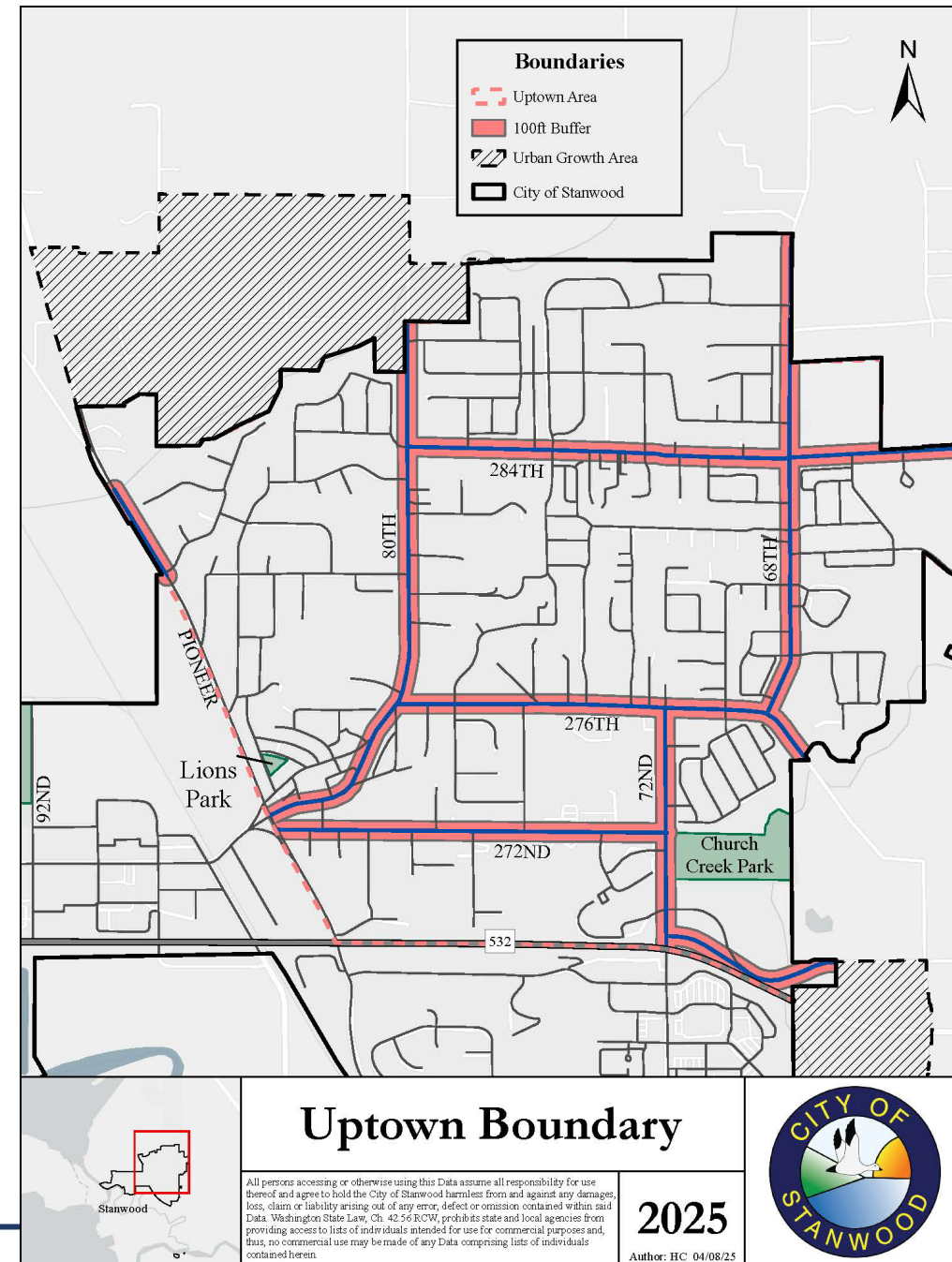
Portions of Pioneer Highway

### ■ Meet the Following:

- In the Uptown Area (North of SR 532)

And:

- Located on a Corner Lot; or
- At Least a Half Acre



# Neighborhood Small-Scale Commercial



## Proposal: Potential Uses

Land Use	Maximum Floor Area (square feet)
Retail Shop/Boutique	3,000
Coffee Shop/Bakery	3,000
Café/Bistro	3,000
Live-Work Units	3,000 (non-residential area)
Salon	3,000
Animal Daycare/Grooming	3,000
Veterinarian Hospital or Clinic (No outdoor kennels allowed)	5,000
Professional Office	5,000
Healthcare Office	5,000
<b>Daycare Center*</b> (Subject to SMC 17.95.382)	6,000
Preschool	6,000

\* New State Law: Daycare Centers Must Be Allowed in all Residential and Commercial Zones



# Neighborhood Small-Scale Commercial

## Proposal: Standards

- Oriented To The Street
- Window Transparency
- 5 Foot Setback
- Landscaping / Buffer / Fencing
- Limitations on Signage
- Off-Street Parking
  - Reductions Allowed w/ Study



No:  
Covered Windows  
Advertising  
Sign Clutter



Yes:  
Transparent Windows  
Outdoor Seating  
Landscape (Pots)

# Neighborhood Small-Scale Commercial

## Proposal: Neighborhood Compatibility

### *Prohibited Uses:*

- Deep Fryers – Odor
- Open Flame Grill Tops – Odor
- Generators / HVAC Systems Exceeding 55 Decibels – Noise
- Amplified Sound Systems / Outdoor Speakers - Noise
- Machinery / Equipment Exceeding 55 Decibels – Noise

**Review Process:** Administrative or Public Hearing?

**Alternatively:** Optional Impact Study Analysis



Seattle

# Neighborhood Small-Scale Commercial

## Proposal: Neighborhood Fear

Run Down Buildings  
Big Signs  
Unwelcoming Façade



Everett



Size and Scale of Building  
Traffic

# Neighborhood Small-Scale Commercial

## Proposal: Examples

- Conversion of a single-family home into a café, gallery, boutique, or small office;
- Construction of a small-scale corner commercial building;
- Small-scale mixed-use building with ground-floor commercial use and a residential use above;
- A live/work unit with a customer-facing commercial use on the ground floor; or
- Reuse of historic or vacant building for a neighborhood focused business.



# Neighborhood Small-Scale Commercial



## Proposal: Social Media Comments (115)

### Support

- Strong Nostalgia for Corner Stores
- Widespread Support for Cafes, Childcare and Community Amenities
- Emphasis on Walkability and Small-Town Feel
- Desire for More Entertainment & Dining Options (Not Necessarily in Uptown Area)

### Concerns

- Fear of Overdevelopment and Urbanization
- Strong Concern About Traffic and Overcrowding
- Slow Down Growth – Keep Agricultural Roots
- Distrust of City



# Neighborhood Small-Scale Commercial



## Proposal: Social Media Comments (115)

### **Notable Suggestions:**

- Support small, local businesses only—not chains.
- Avoid urban mixed-use models in residential zones.

### **Other Suggestions – Not Related to Corner Stores:**

- Explore themes for Stanwood (like Leavenworth or Poulsbo) to attract tourism.
- Fund community centers for youth and families.



# Neighborhood Small-Scale Commercial

## Questions / Suggestions?



Family Market and Deli - Everett

**Small Scale Commercial Uses in Uptown Neighborhood  
Feedback Summary**

**Small Scale Commercial Development**  
**“Corner Store”**  
**Comment Summary**  
**(Generated by AI from Over 450 Comments)**

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**Purpose**

The purpose of this staff report is to summarize community feedback gathered through a virtual open house, in-person open house, and forthcoming social media engagement regarding proposed updates to the municipal code. These updates would allow small-scale neighborhood amenities—such as corner markets, cafes, childcare centers, and boutiques—in residential areas throughout Stanwood.

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**Engagement Summary**

The City hosted a virtual open house and an in-person open house on Thursday, May 29, and two rounds of social media outreach to gather feedback from community members. This report summarizes input.

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## Key Themes from Open House Community Feedback

### 1. General Support for Small-Scale Amenities

- Mixed support for allowing neighborhood-friendly businesses such as cafes, corner markets, boutique shops, and childcare centers.
- Many residents expressed a desire for walkable community spaces with local character and small-scale commercial services similar to Cedarhome Market or Elger Bay Café.
- Live/work opportunities received positive feedback, especially to enhance flexibility and economic vibrancy within neighborhoods.

### 2. Walkability, Safety, and Infrastructure Concerns

- Several residents highlighted the importance of improving pedestrian infrastructure to support walkable neighborhoods:
  - Lack of continuous sidewalks and slippery/mossy conditions were noted as barriers.

- Desire for more garbage cans, dog waste stations, and places to rest (benches).
- Traffic calming and pedestrian safety improvements—especially near informal walking routes—were frequently mentioned.

### **3. Concerns About Compatibility and Neighborhood Character**

- Support was conditional on ensuring businesses remain small, low-impact, and integrated into the neighborhood fabric:
  - Clear preferences for local, independent-style businesses over chains or convenience stores (e.g., “no 7-Eleven”).
  - Emphasis on noise control, lighting, odor, and dust mitigation.
  - Requests to preserve neighborhood history and integrate wayfinding and cultural markers to reflect Stanwood’s identity.

### **4. Operational and Regulatory Preferences**

- Participants advocated for a streamlined permitting process:
  - Mixed opinions on review process:
    - Some were supportive of administrative review and approval over public hearings to avoid excessive barriers.
    - Others thought a tiered system may be appropriate with more impactful uses, such as a café, should require a hearing.
- Some support for relaxed parking standards to reduce development barriers for small businesses.
- Mixed views on use types: enthusiasm for cafes, pubs with family-friendly spaces, indoor play cafes, and catteries; opposition to automotive repair and high-traffic uses (e.g., healthcare centers).

### **5. Equity and Safety Considerations**

- Some participants expressed concerns about increased theft or transient activity, particularly referencing other cities (e.g., Everett).

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## **Conclusion and Recommendations**

Public feedback generally supports the concept of allowing neighborhood-scale amenities, provided that development is limited and carefully tailored to maintain the character and livability of residential areas

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# Key Findings From Social Media Outreach (May 12 and May 26)

Topic: Proposed updates to Stanwood's municipal code to allow small-scale amenities—such as corner markets, boutiques, childcare centers, and cafes—in residential areas.

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## Key Themes of Support

- **Strong Favor for Corner Stores & Cafes**
  - "Bring back the corner store" was a recurring phrase, with many citing fond memories of Cedarhome Market, the Utsalady store, and others.
  - Cafes and small shops are seen as enhancing community life and reducing the need for vehicle trips.
- **Childcare as a Priority**
  - Multiple comments highlighted an urgent need for more childcare and preschool options—especially given current population growth.
  - This was a top concern for young families and echoed across both comment threads.
- **Local, Independent Businesses Preferred**
  - Residents clearly distinguished support for *mom-and-pop* businesses over corporate chains.
  - There was concern that code changes might pave the way for franchises (e.g., Starbucks), which many oppose.
- **Examples from Other Communities**
  - Residents cited small neighborhood stores and hangouts from Anacortes, Spokane, Shoreline, and Camano as positive models.
  - Several also referenced past Stanwood institutions like Gargoyles and The Spot, suggesting a desire to reclaim that neighborhood-oriented energy.

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## Concerns and Reservations

- **Traffic, Roads, and Parking**

- Numerous residents expressed worry that neighborhood-scale businesses could add congestion to already strained roads.
  - Several raised the need for street upgrades and traffic safety improvements before allowing more development.
  - **Preserving Small-Town & Rural Character**
    - "Keep the country, country" and similar sentiments were echoed frequently.
    - Many fear overdevelopment and creeping urbanization, citing Mill Creek and Lynnwood as cautionary tales.
  - **Distrust of Overdevelopment & Rezoning**
    - Concerns about high-density mixed-use zoning emerged, particularly opposition to adding apartments or condos in residential neighborhoods.
    - Some respondents mistrust the city's motives and fear changes will benefit developers more than residents.
  - **Strain on Schools and Services**
    - A common refrain was that new growth should be accompanied by increased school capacity, fire protection, and other essential services.
    - Some suggested impact fees or developer requirements to mitigate these effects.
- 

## Other Trends & Takeaways

- **Walkability and Community Connectivity**
  - Walkability was highly valued. Several respondents asked for better sidewalks, benches, and pedestrian infrastructure—especially for seniors and young families.
- **Desire for More Youth and Teen Spaces**
  - Many shared memories of teen-friendly businesses like The Spot and Gargoyles and expressed a desire to bring back safe hangouts for youth.
- **Split Opinions on Growth**
  - Some support moderate growth and adaptation to changing needs; others see the initiative as a threat to Stanwood's identity.
- **Appreciation for Existing Businesses**

- Cedarhome Market was repeatedly cited as a cherished neighborhood asset. Residents want to preserve and replicate its success, not replace it with modern alternatives.

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### **Overall Summary / Key Takeaway from Social Media Feedback**

Community responses showed **broad support for allowing small-scale, local-serving businesses in neighborhoods**—such as corner stores, cafes, and childcare centers—**as long as they maintain Stanwood’s small-town character** and do not lead to overdevelopment or the introduction of large commercial chains.

Residents strongly value:

- **Walkability and neighborhood connectivity**
- **Support for local, independent businesses**
- **Reviving nostalgic and community-oriented gathering places**

However, support is **tempered by significant concerns** about:

- Traffic and infrastructure capacity
- Impact on schools, safety, and services
- Distrust of rezoning that could lead to dense urban development

In short, the public is **open to neighborhood businesses—but not at the expense of Stanwood’s identity, infrastructure, or rural charm**. Thoughtful, scaled, and locally focused implementation would likely receive the most community support.

**Stanwood Active Transportation Level of Service Evaluation**

## MEMORANDUM

<b>Date:</b>	May 19, 2025	<b>TG:</b>	21010.00
<b>To:</b>	Patricia Love, Shawn Smith – City of Stanwood		
<b>From:</b>	Paul Sharman, Patrick Lynch – Transpo Group		
<b>cc:</b>	Stanwood PW Committee, Stanwood CDC		
<b>Subject:</b>	Stanwood Active Transportation Level of Service Evaluation		

Transpo Group is currently supporting the City of Stanwood with the development of a multimodal level of service (LOS) standard for evaluating the City’s transportation network. The following memorandum outlines the City’s vision for the active transportation network, as well as potential approaches and standards for evaluating the roadways within that network.

### ***Active Transportation Level of Service***

#### **Active Transportation Network Vision**

The City’s vision for the future roadway network is to provide active mode facilities on all roadways unless special circumstances make it prohibitive. The City’s Transportation Improvement Plan identifies numerous projects to install sidewalks to improve access for active transportation users along roadways within the City that currently lack active mode facilities.

As part of new development projects, the City requires that sidewalk facilities be constructed along internal streets and adjacent frontages in accordance with the design standards outlined in the City of Stanwood’s Street and Utility Standards. This has helped the City to achieve parts of the active transportation vision; however, reliance on developer-implemented improvements alone would result in gaps within the bicycle and pedestrian network.

The Active Transportation Plan Network, shown in Figure 1, identifies the future vision for a comprehensive network of active transportation facilities. The City envisions an interconnected system of on-road and off-road facilities, that include sidewalks, pathways, shared-use trails, and key connections.

The active transportation network contains a series of Primary or Secondary Routes. Corridors identified as Primary or Secondary Routes are not indicative of a hierarchy for future active transportation facility development, rather they are used to make a distinction between routes that are more regional or that extend completely through the community (primary), and those that serve to make the second leg of the journey to connect to destinations, extend into neighborhoods, or complete a loop (secondary). Table 1 further defines the functions of each tier within the Active Transportation Plan Network.

Note that the active transportation network does not differentiate between pedestrian and bicycle facilities, with the primary focus of this network being pedestrian connectivity. Due to the limited resources available for the Comprehensive Plan update process, additional work will be required as part of a separate effort to refine this network for bicycle connectivity. The City’s vision for the bicycle network can build of that shown in Figure 1 and could be prepared as part of the development of a future Pedestrian and Bicycle Master Plan study. This study would provide greater detail and planning around mobility for pedestrian and bicycle modes throughout the City.

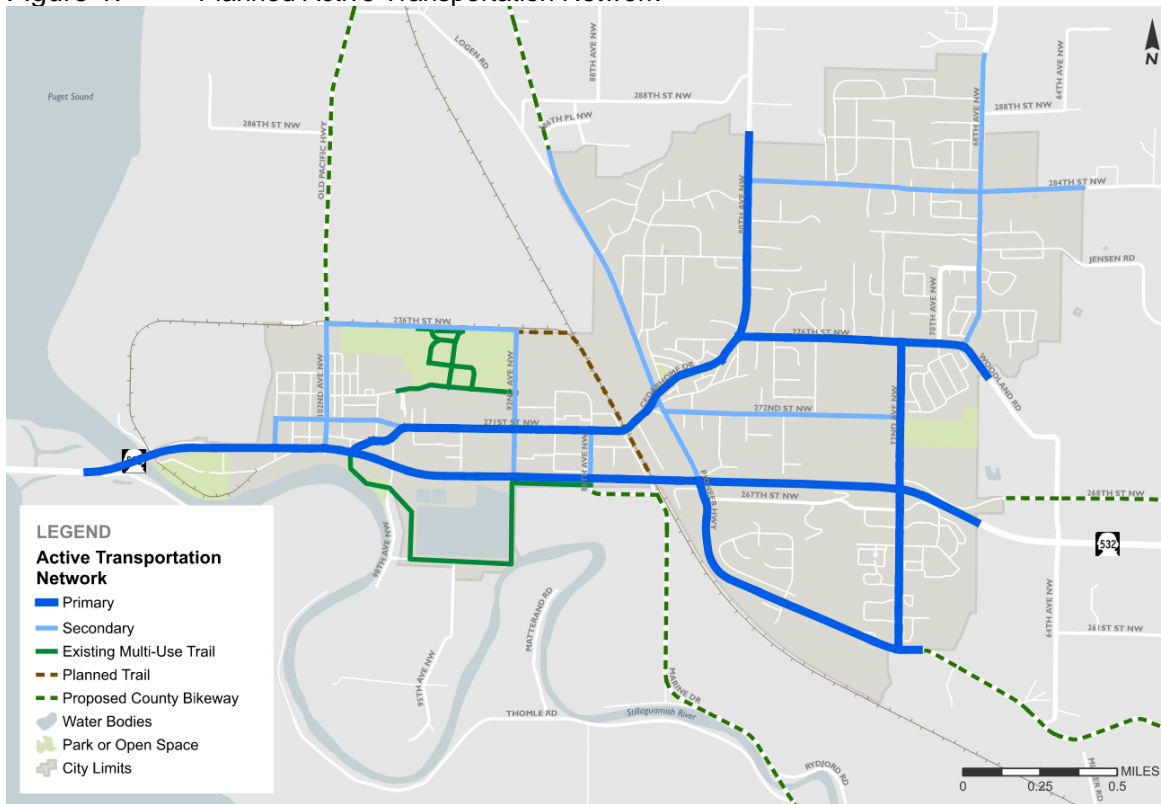
**Table 1. Table 1 – Active Transportation Plan Network Definitions**

Hierarchy	System Function
Primary Network	Backbone of the system. Offers direct connections to majority of important community destinations, usually on arterials or collectors. Primary Network routes are often the most attractive route in terms of convenience in urban areas.
Secondary Network	Supportive to the Primary Network, often providing system continuity by connecting segments of the primary network with on-street or off-street facilities. Secondary Network routes sometimes offer more comfortable routes on quieter streets, although the route may not be as direct as the Primary network.
Other Streets	This encompasses the majority of streets within the City (including residential neighborhood streets). While not specifically identified within the Active Transportation Plan Network, many of these roadways provide pedestrian and/or bicycle facilities in line with roadway design standards. Other Streets provide access to the Primary and Secondary Networks.
Off-Street Multiuse Trails	Trails represent the off-street pedestrian and bicycle facilities within the City. Trails often provide the direct connectivity of Primary Network routes but are located along alignments away from roadways (creating a more comfortable pedestrian and bicycle environment).

It should be noted that the roadways within the Active Transportation Plan Network are not the only roadways within the City designated to have pedestrian/bicycle facilities. As indicated in the Stanwood Street and Utility Standards, most roadways within the City are required to provide some level of pedestrian facilities.

The Active Transportation Network is not meant to define the type, width, and locations of these facilities for Primary and Secondary Network roadways. Instead, the roadway design standards designate the types of facilities planned for each roadway based on its functional classification. The Active Transportation Plan Network is intended to lay out the active transportation vision for the City, not to designate the sidewalk and bicycle facilities for each roadway.

Figure 1. Planned Active Transportation Network



## Level of Service Evaluation Approaches

Various approaches can be taken to evaluate active transportation and assign a LOS value for roadways within the City. The approaches can range from simple to complex and require varying levels of data to implement. Two potential approaches which can be implemented for Stanwood have been outlined below. Benefits and drawbacks of each approach have also been provided to aid in selecting the preferred approach to implement.

The active transportation system performance standards outlined below do not replace the City's existing multimodal street policies and design standards, which require the installation of pedestrian and/or bicycle facilities along new or improved roadways. Instead, these standards are intended to guide the retrofit of older roadway infrastructure that was built with inadequate active transportation facilities (prior to the adoption of the existing design standards) to meet current expectations for system performance as it relates to bicycle and pedestrian travel.



### ***Approach 1: Consistency with Design Standards***

The first approach would be to evaluate the active transportation LOS based on the consistency of each roadway's pedestrian facilities with the roadway design standards corresponding to the functional classification of the roadway. This approach uses consistency with the City's Street and Utility Standards to evaluate each roadway instead of based on the presence of facilities on one vs. two sides of the roadway.

For this approach, the LOS standards that would be used to evaluate the active transportation network would be based on consistency with the roadway standards outlined in the City of Stanwood

Street and Utility Standards. The LOS standards are shown in Table 2, along with example facilities and their associated LOS value.

**Table 2. Active Transportation Levels of Service Overview – Approach 1**

LOS	Rating	Standard	Example Facilities*
	Good	Roadway provides pedestrian/bicycle facilities in accordance with standards	<ul style="list-style-type: none"> <li>• Sidewalks on both sides of the roadway</li> <li>• Multi-use path on one side of the roadway</li> </ul>
	Acceptable	Roadway provides pedestrian/bicycle facilities, but does not fully meet standards	<ul style="list-style-type: none"> <li>• Sidewalk along one side of the roadway</li> <li>• On-street shoulder facility for pedestrians/bicyclists</li> </ul>
	Poor	No facilities exist	<ul style="list-style-type: none"> <li>• No facilities exist</li> </ul>

\* Note that the example facilities for each LOS standard will vary based on the roadway functional classification. A facility may indicate a "green/good LOS for one functional classification but an orange/acceptable LOS for another.

The active transportation network has been identified through a series of Primary or Secondary Routes. Corridors identified as **Primary** or **Secondary Routes** are not indicative of a hierarchy for future active transportation facility development, rather they are used to make a distinction between routes that are more regional or that extend completely through the community (primary), and those that serve to make the second leg of the journey to connect to destinations, extend into neighborhoods, or complete a loop (secondary). As Primary and Secondary Network roadways identified within the Active Transportation Plan work together to achieve the active transportation vision for the City, a single set of LOS standards was developed to apply to the roadways within both networks.

The LOS standards shown in Table 2 emphasize system completion of sidewalks, pathways, or multi-use trails on arterial and collector roadways, or along off-street corridors. The LOS designations are shown in green, orange, and red corresponding with good, acceptable, and poor LOS, respectively. The long-term vision for the City would be to have the Primary and Secondary Network roadways achieve a green or good LOS; however, in the near-term, the objective would be to achieve, at minimum, an orange or acceptable LOS along these roadways.

Generally, a green/good LOS indicates a roadway provides the design specified pedestrian facilities for that functional classification, while an orange/acceptable LOS indicates that a pedestrian facility is provided but does not meet the appropriate design standard. A red/poor LOS generally indicates no designated facilities are provided for active transportation users and is considered unacceptable.

Note that the example facilities provided in Table 2 may correspond with different LOS values based on the functional classification of the roadway. For example, a sidewalk on both sides of the roadway without bicycle lanes would align with the roadway standard for Collectors (corresponding with a green/good LOS) but would not fully meet the roadway design standard for an Arterial (corresponding with an orange/acceptable LOS).

The benefits and drawbacks of taking this approach to evaluate active transportation LOS are summarized below:

#### Approach Benefits

- Simple to implement/update as projects get completed
- Requires few data sources for evaluation
- Can apply to all roadways based on alignment with roadway standards
- Provides a structured, objective way to evaluate the LOS of each roadway
- Clearly defines the improvements required to improve the LOS
- Identifies specific locations where improvements are necessary

#### Approach Drawbacks

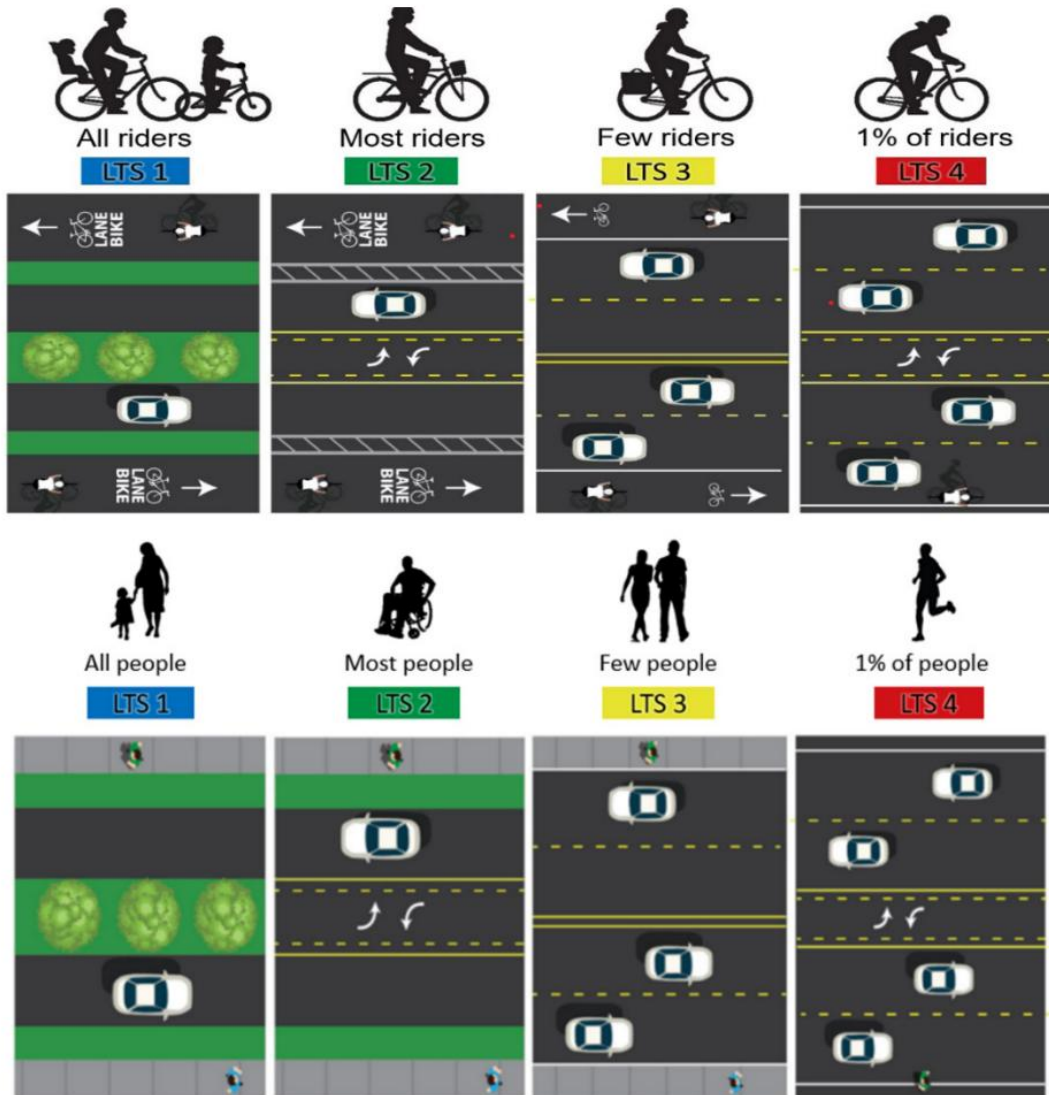
- Provides a simplistic evaluation of the pedestrian/bicycle network
- Does not evaluate the level of comfort for pedestrian/bicycle use
- Changes to roadway standards would require the LOS evaluation to be updated

### **Approach 2: Level of Traffic Stress Evaluation**

The second approach would be to evaluate the active transportation LOS based on the level of traffic stress (LTS) along each roadway within the Active Transportation Plan Network. This approach would provide a more direct measurement of the pedestrian/bicycle network based on how comfortable each facility feels to use.

Each roadway identified on the planned Active Transportation Network would be evaluated and scored based on their pedestrian and bicycle LTS. According to the Washington State Department of Transportation (WSDOT), LTS is “a ranking system where level 1 feels safe and comfortable for all users, while levels 3 and 4 represent stressful conditions that many people will not or cannot use. WSDOT’s LTS rankings are informed by the Safe System Approach such that the probability of a serious injury or fatality is decreased in the event of a crash.” As such, WSDOT uses a target LTS 1 or 2 for their design standards. LTS rankings 1 through 4 are illustrated in Figure 2.

Figure 2. Level of Traffic Stress Illustration

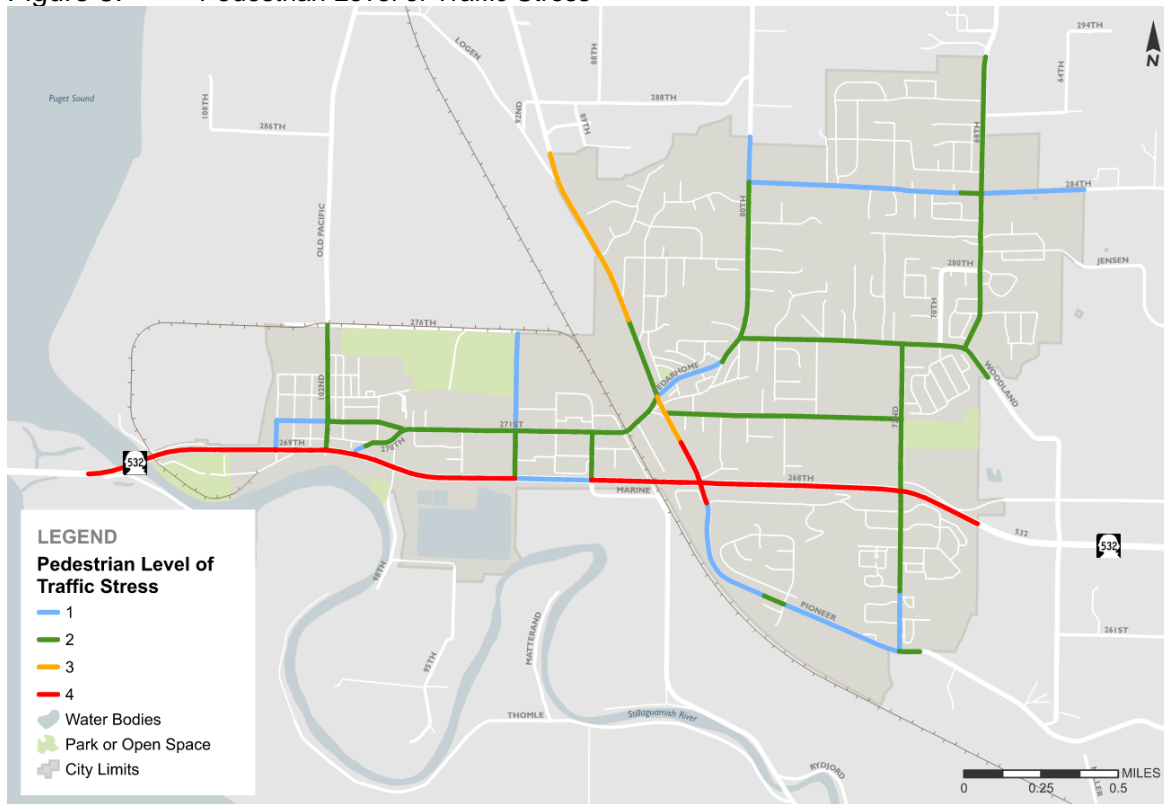


WSDOT has developed a methodology to score LTS based on roadway traffic volumes, roadway speeds, and the presence of active transportation facilities<sup>1</sup>. This methodology was directly leveraged to score the LTS along SR 532. However, the LTS methodology WSDOT has developed was specifically done for state highways, and does not translate well to local city streets. As such, the WSDOT LTS Methodology was tweaked to better reflect conditions on Stanwood roadways. The complete table of LTS scores is shown in Attachment A.

**A level of service methodology based on LTS is as simple as stating LTS 1 or 2 as acceptable, and LTS 3 or 4 as unacceptable.** The existing pedestrian LTS in Stanwood is shown in Figure 3.

<sup>1</sup> [https://wsdot.wa.gov/sites/default/files/2024-07/LTS%20Level%20of%20Traffic%20Stress%20Flyer\\_0.pdf](https://wsdot.wa.gov/sites/default/files/2024-07/LTS%20Level%20of%20Traffic%20Stress%20Flyer_0.pdf)

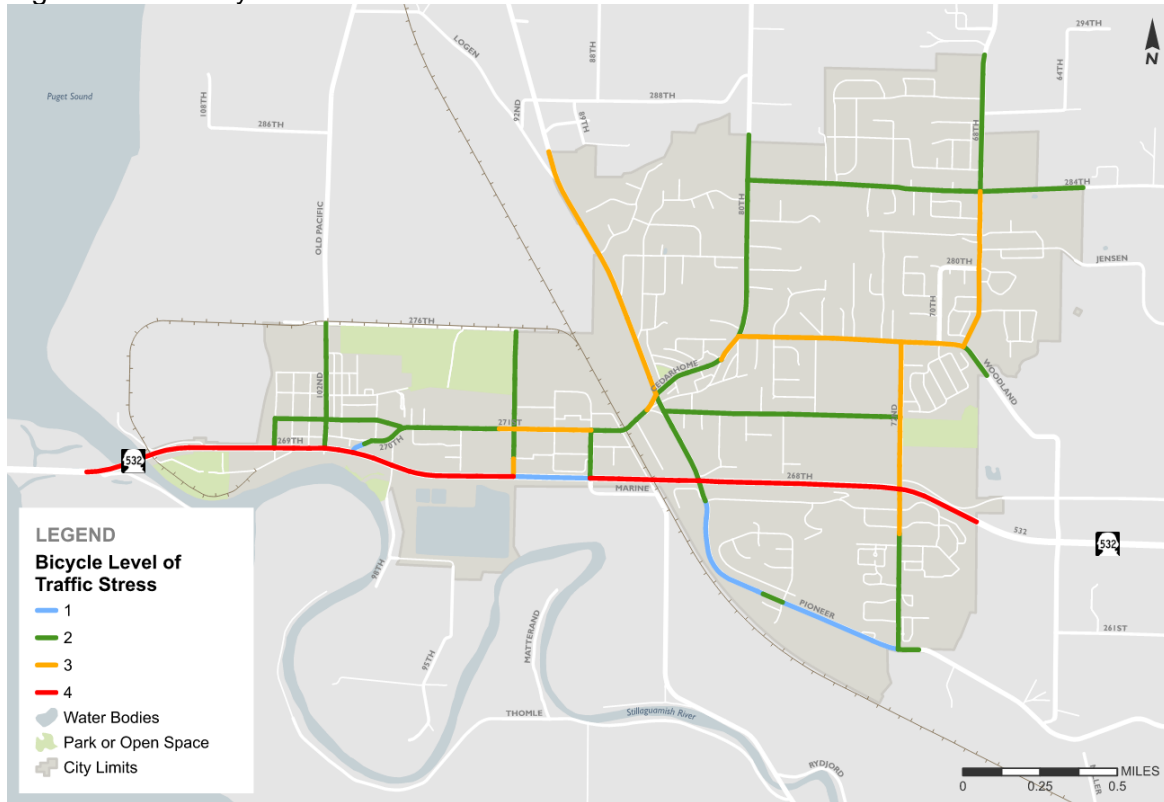
Figure 3. Pedestrian Level of Traffic Stress



Generally, pedestrian LTS is scored at 1 or 2 along most City roadways, largely due to the presence of sidewalks on many city streets. Most of SR 532 has a high LTS due to the lack of facilities, high speeds and high traffic volumes. Some sections of Pioneer Hwy are also scored at LTS 3 or 4. Capital projects to mitigate high pedestrian LTS could include sidewalk installation or construction of off-street multiuse paths.

Existing Bicycle LTS is shown in Figure 4.

Figure 4. Bicycle Level of Traffic Stress



Existing Bicycle LTS in Stanwood ranges from 1 to 3 across City streets, while SR 532 scores 4. Despite Stanwood’s current lack of bicycle infrastructure on city streets, many roadways have a bicycle LTS 2 because of low speeds and volumes – indicating that a shared roadway is appropriate for many city streets. Roadways with LTS 3 and 4 would need to better accommodate bicyclists under this methodology and improvements could include speed reductions, adding bicycle lanes, or off-street shared use paths.

The benefits and drawbacks of using the LTS methodology to evaluate active transportation LOS are summarized below:

Approach Benefits

- Provides a more comprehensive evaluation of the active transportation network
- Compatible with WSDOT and general industry best practice
- Incorporates an evaluation of the pedestrian/bicycle comfort levels
- Would not be affected by changes to roadway standards

Approach Drawbacks

- Requires more data sources to fully evaluate each roadway
- Requires a more detailed review and evaluation of each corridor to assign a LOS value
- Results in more roadway LOS ‘failures’

## **Next Steps**

After Stanwood has selected a preferred methodology for active transportation LOS evaluation, Transpo Group will assist the City with implementing the methodology. This will include amending the Transportation Element (TE) of the comprehensive plan to include the new LOS standards, updating the 20-year capital project list within the TE, and updating the City's transportation impact fees (TIFS) to include the new capital projects identified.

# PEDESTRIAN LEVEL OF TRAFFIC STRESS

NO PEDESTRIAN FACILITY						
Lanes	ADT	Speed Limit				
		≤20	25	30	35	≥40
2 lanes	0-750	1	1	3	4	4
	750-1,500	1	2	3	4	4
	1,500-3,000	2	2	3	4	4
	3,000-4,500	3	3	4	4	4
	4,500-6,000	3	3	4	4	4
	6,000+	3	3	4	4	4
3 lanes	0-1,500	1	2	3	4	4
	1,500-3,000	2	3	3	4	4
	3,000-6,000	2	3	4	4	4
	6,000-9,000	3	4	4	4	4
	9,000-12,000	3	4	4	4	4
	12,000+	3	4	4	4	4
4 lanes	0-2,000	3	3	3	4	4
	2,000-4,000	3	3	4	4	4
	4,000-8,000	3	4	4	4	4
	8,000-12,000	3	4	4	4	4
	12,000-16,000	4	4	4	4	4
	16,000+	4	4	4	4	4
5 lanes	0-3,000	3	3	4	4	4
	3,000-6,000	3	4	4	4	4
	6,000-12,000	3	4	4	4	4
	12,000-18,000	4	4	4	4	4
	18,000-24,000	4	4	4	4	4
	24,000+	4	4	4	4	4

SIDEWALK (5' TO 7.5' WIDE) WITHOUT BUFFER <sup>1</sup>						
Lanes	ADT	Speed Limit				
		≤20	25	30	35	≥40
2 lanes	0-750	1	1	2	3	4
	750-1,500	1	1	2	3	4
	1,500-3,000	1	2	2	3	4
	3,000-4,500	2	2	2	3	4
	4,500-6,000	2	2	2	3	4
	6,000+	2	2	2	3	4
3 lanes	0-1,500	1	1	2	3	4
	1,500-3,000	1	2	2	3	4
	3,000-6,000	2	2	2	3	4
	6,000-9,000	2	2	2	3	4
	9,000-12,000	2	2	3	3	4
	12,000+	2	2	3	4	4
4 lanes	0-2,000	2	2	2	3	4
	2,000-4,000	2	2	2	3	4
	4,000-8,000	2	2	3	4	4
	8,000-12,000	2	2	3	4	4
	12,000-16,000	2	2	3	4	4
	16,000+	2	2	3	4	4
5 lanes	0-3,000	2	2	2	3	4
	3,000-6,000	2	2	2	3	4
	6,000-12,000	2	2	3	4	4
	12,000-18,000	2	2	3	4	4
	18,000-24,000	2	2	3	4	4
	24,000+	2	2	3	4	4

<sup>1</sup>"Without Buffer" indicates sidewalk is directly adjacent to the travel lane (i.e., no parking lane or bicycle lane)

# PEDESTRIAN LEVEL OF TRAFFIC STRESS

## SIDEWALK (8' WIDE OR MORE) AND/OR WITH BUFFER<sup>2</sup>

Lanes	ADT	Speed Limit				
		≤20	25	30	35	≥40
2 lanes	0-750	1	1	2	2	3
	750-1,500	1	1	2	2	3
	1,500-3,000	1	1	2	2	3
	3,000-4,500	1	2	2	2	3
	4,500-6,000	1	2	2	2	3
	6,000+	1	2	2	2	3
3 lanes	0-1,500	1	1	2	2	3
	1,500-3,000	1	1	2	2	3
	3,000-6,000	1	2	2	2	3
	6,000-9,000	1	2	2	2	3
	9,000-12,000	2	2	2	3	3
	12,000+	2	2	3	3	3
4 lanes	0-2,000	1	2	2	2	3
	2,000-4,000	1	2	2	2	3
	4,000-8,000	2	2	2	2	3
	8,000-12,000	2	2	2	3	3
	12,000-16,000	2	2	3	3	4
	16,000+	2	2	3	3	4
5 lanes	0-3,000	1	2	2	2	3
	3,000-6,000	2	2	2	2	3
	6,000-12,000	2	2	2	3	3
	12,000-18,000	2	2	3	3	4
	18,000-24,000	2	2	3	4	4
	24,000+	2	2	3	4	4

<sup>2</sup>"With Buffer" indicates that there is separation between the sidewalk and travel lane.

This can be achieved through a striped shoulder, parking lane, or bicycle lane (minimum 3-foot width)

## MULTI-USE PATH (MORE THAN 2 FEET OF ROBUST SEPARATION<sup>3</sup>)

Lanes	ADT	Speed Limit				
		≤20	25	30	35	≥40
ANY	0-3,000	1	1	1	2	2
	3,000-6,000	1	1	1	2	2
	6,000-12,000	2	2	2	2	2
	12,000+	2	2	2	2	2

<sup>3</sup>"Multi-Use Path" must be minimum 10 feet in width

"Robust Separation" refers to indicates the presence of buffer between the pathway and vehicle travel lane which provides greater protection to the bicyclist (generally is grade-separated by a curb, ditch, dike, berm, etc.).

# BICYCLE LEVEL OF TRAFFIC STRESS

**SHARED ROADWAY W/O TRAFFIC CALMING**

Lanes	ADT	Speed Limit				
		≤20	25	30	35	≥40
2 lanes	0-750	1	2	3	4	4
	750-1,500	2	2	3	4	4
	1,500-3,000	2	2	3	4	4
	3,000-4,500	2	3	3	4	4
	4,500-6,000	2	3	3	4	4
	6,000+	2	3	3	4	4
3 lanes	0-1,500	1	2	3	4	4
	1,500-3,000	2	2	3	4	4
	3,000-6,000	2	2	3	4	4
	6,000-9,000	2	3	3	4	4
	9,000-12,000	2	3	4	4	4
	12,000+	2	3	4	4	4
4 lanes	0-2,000	3	3	3	4	4
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	4,000-8,000	3	3	4	4	4
	8,000-12,000	3	4	4	4	4
	12,000-16,000	4	4	4	4	4
	16,000+	4	4	4	4	4
5 lanes	0-3,000	3	3	3	4	4
	3,000-6,000	3	3	4	4	4
	6,000-12,000	3	4	4	4	4
	12,000-18,000	4	4	4	4	4
	18,000-24,000	4	4	4	4	4
	24,000+	4	4	4	4	4

**SHARED ROADWAY W/ TRAFFIC CALMING**

Lanes	ADT	Speed Limit				
		≤20	25	30	35	≥40
2 lanes	0-750	1	1	2	3	4
	750-1,500	1	1	2	3	4
	1,500-3,000	1	2	2	3	4
	3,000-4,500	2	2	3	3	4
	4,500-6,000	2	2	3	3	4
	6,000+	2	3	3	4	4
3 lanes	0-1,500	1	1	2	3	4
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	3,000-6,000	2	2	3	4	4
	6,000-9,000	2	2	3	4	4
	9,000-12,000	2	2	3	4	4
	12,000+	2	3	4	4	4
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	4,000-8,000	3	3	4	4	4
	8,000-12,000	3	4	4	4	4
	12,000-16,000	4	4	4	4	4
	16,000+	4	4	4	4	4
5 lanes	0-3,000	2	3	4	4	4
	3,000-6,000	3	3	4	4	4
	6,000-12,000	3	4	4	4	4
	12,000-18,000	4	4	4	4	4
	18,000-24,000	4	4	4	4	4
	24,000+	4	4	4	4	4

# BICYCLE LEVEL OF TRAFFIC STRESS

## BIKE LANE/SHOULDER W/O SEPARATION

Lanes	ADT	Speed Limit				
		≤20	25	30	35	≥40
2 lanes	0-750	1	1	2	2	3
	750-1,500	1	1	2	2	3
	1,500-3,000	1	2	2	3	3
	3,000-4,500	2	2	2	3	4
	4,500-6,000	2	2	2	3	4
	6,000+	2	2	3	3	4
3 lanes	0-1,500	1	2	2	2	3
	1,500-3,000	1	2	2	3	3
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	6,000-12,000	3	3	3	4	4
	12,000-18,000	3	3	4	4	4
	18,000-24,000	3	4	4	4	4
	24,000+	4	4	4	4	4

## BIKE LANE/SHOULDER W/ SEPARATION

Lanes	ADT	Speed Limit				
		≤20	25	30	35	≥40
2 lanes	0-750	1	1	2	2	2
	750-1,500	1	1	2	2	2
	1,500-3,000	1	1	2	2	3
	3,000-4,500	2	2	2	3	3
	4,500-6,000	2	2	2	3	3
	6,000+	2	2	2	3	3
3 lanes	0-1,500	1	1	2	2	2
	1,500-3,000	1	1	2	2	3
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	18,000-24,000	2	3	4	4	4
	24,000+	2	3	4	4	4

## MULTI-USE PATH (MORE THAN 2 FEET OF ROBUST SEPARATION<sup>1</sup>)

Lanes	ADT	Speed Limit				
		≤20	25	30	35	≥40
ANY	0-3,000	1	1	1	2	2
	3,000-6,000	1	1	1	2	2
	6,000-12,000	2	2	2	2	2
	12,000+	2	2	2	2	2

<sup>1</sup>"Multi-Use Path" must be minimum 10 feet in width

"Robust Separation" refers to indicates the presence of buffer between the pathway and vehicle travel lane which provides greater protection to the bicyclist (generally is grade-separated by a curb, ditch, dike, berm, etc.).

## **Multimodal Level of Service Presentation**

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# MULTIMODAL LEVEL OF SERVICE (MMLoS)

**City of Stanwood  
(06/02/25)**

transpogroup   
WHAT TRANSPORTATION CAN BE.

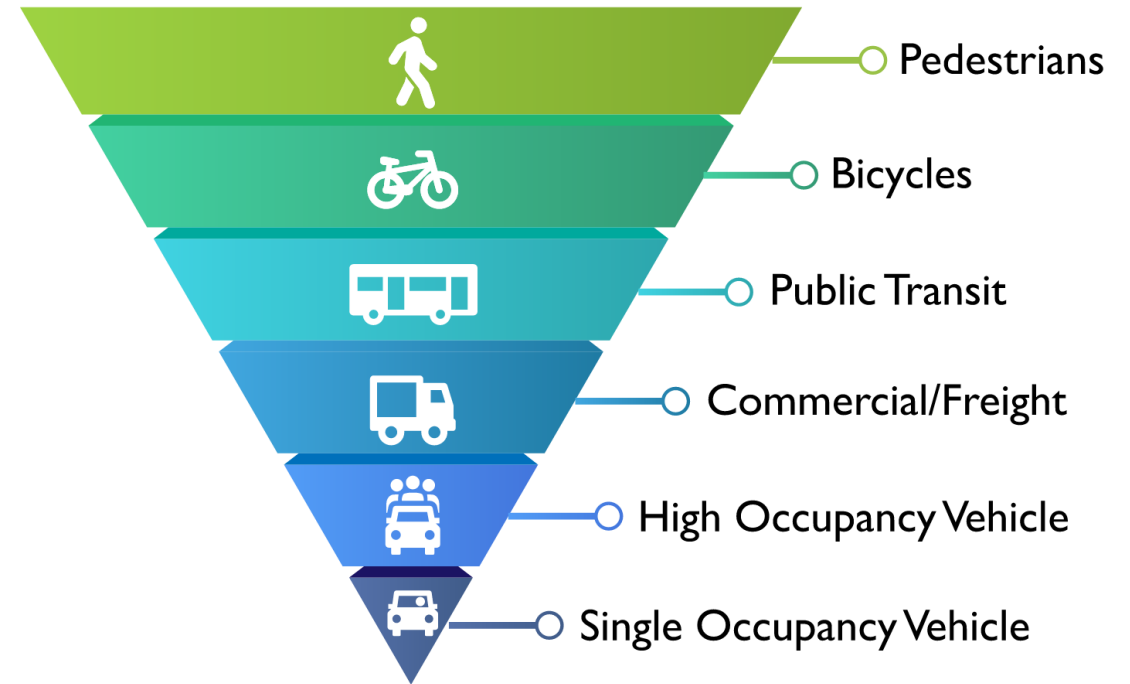
# PRESENTATION OUTLINE

- Why are we doing this? (state and PSRC requirements)
- What is Level of Service?
- What options are we proposing in Stanwood?
  1. Roadway Design Standard Based
  2. Level of Traffic Stress (LTS) Based
- Next steps

WHY ARE WE  
DOING THIS?

# GROWTH MANAGEMENT ACT (GMA)

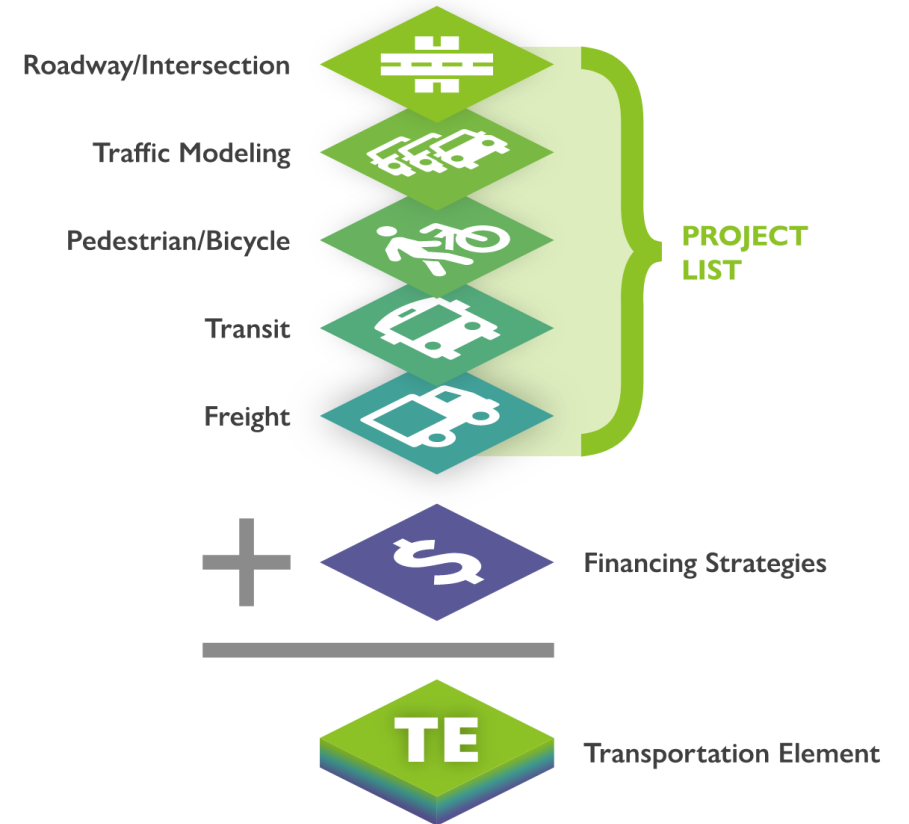
- RCW 36.70A.070 Comprehensive Plans – Mandatory Elements
- “The plan shall be an internally consistent document and all elements shall be consistent with the future land use map.”
- (6) “A transportation element that implements, and is consistent with, the land use element.”
- (B) “**Multimodal** level of service standards for all locally owned arterials, locally and regionally operated transit routes that serve urban growth areas, state-owned or operated transit routes”



# VISION 2050 PLANNING POLICIES

PSRC Minimum Expectations = Multimodal concurrency and LOS programs that meet GMA requirements and VISION 2050's multicounty planning policies will include:

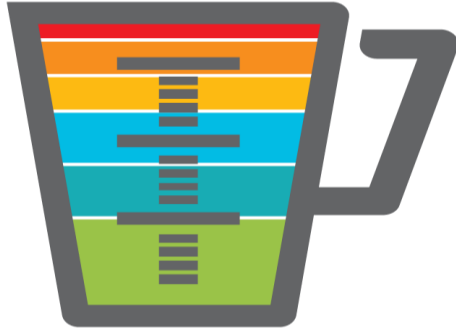
- Methodology to evaluate LOS for transit, bicycles, and pedestrians and vehicles
- Adopt LOS standard(s) based on the methodology
- Identification of existing and future deficiencies – to maintain LOS
- Measures for addressing existing and future deficiencies – to maintain LOS



# WHAT IS LEVEL OF SERVICE?

# TYPICAL VEHICULAR LOS STANDARDS

- Level of service (LOS) is a measure of transportation system performance (A to F)
- Typically measured in terms of vehicle delay or throughput

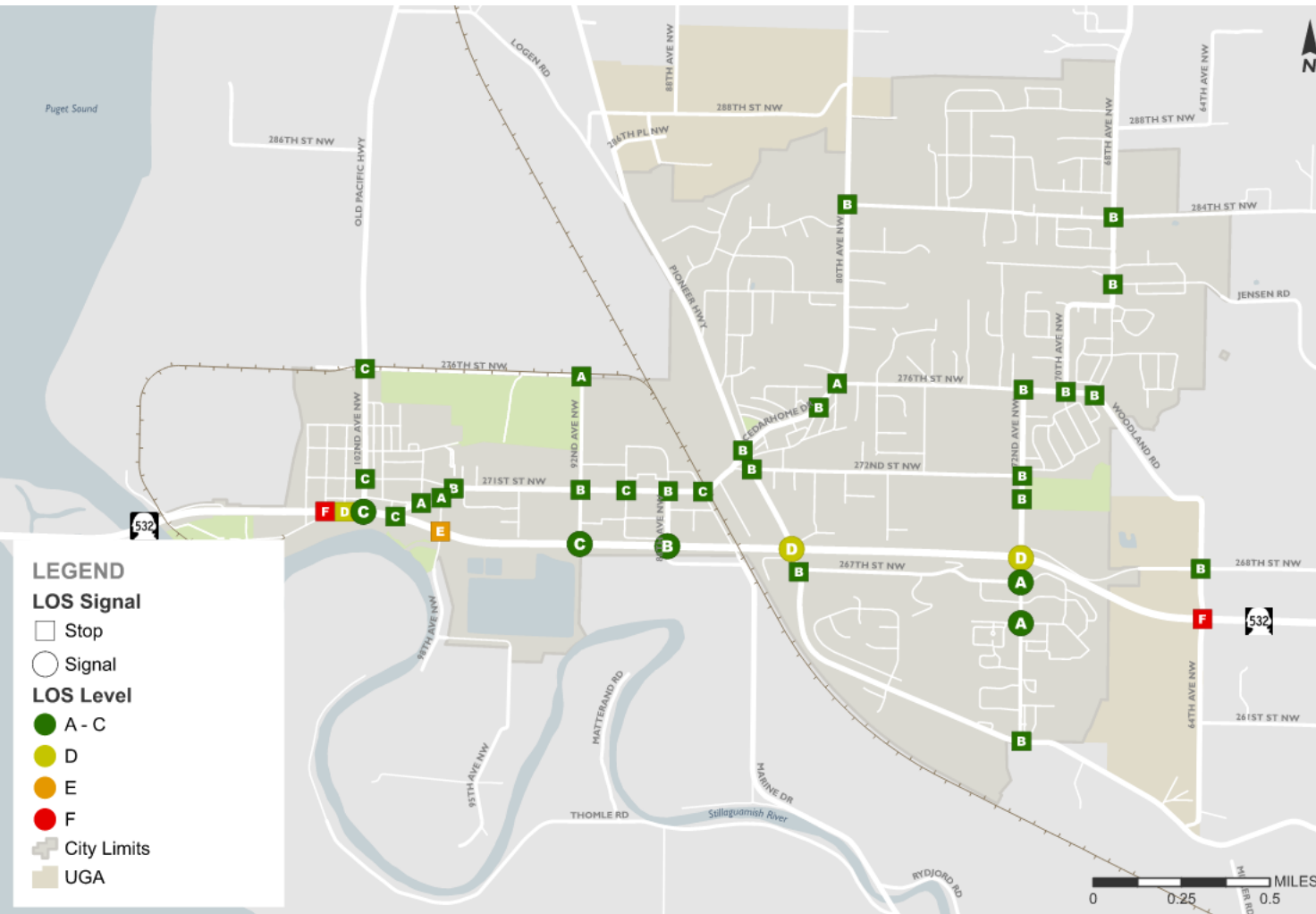


Roadway  
Volume-to-Capacity  
*(vehicle throughput)*



Intersection Delay/  
seconds per vehicle  
*(driver inconvenience)*

# STANWOOD EXISTING LOS STANDARDS



## Intersection Based

### LOS D

- All traffic signals, roundabouts, and all-way stop intersections

### LOS E

- *All two-way stop controlled intersections*

### WSDOT LOS standards for intersections:

- *LOS D or better in urban areas (includes SR 532)*
- *LOS C or better in rural areas*

### **NOT PROPOSING A CHANGE TO VEHICLE LOS**

MMLOS OPTION 1 –

ROAD STANDARD  
BASED

# ROAD STANDARD DESIGN BASED MMLOS

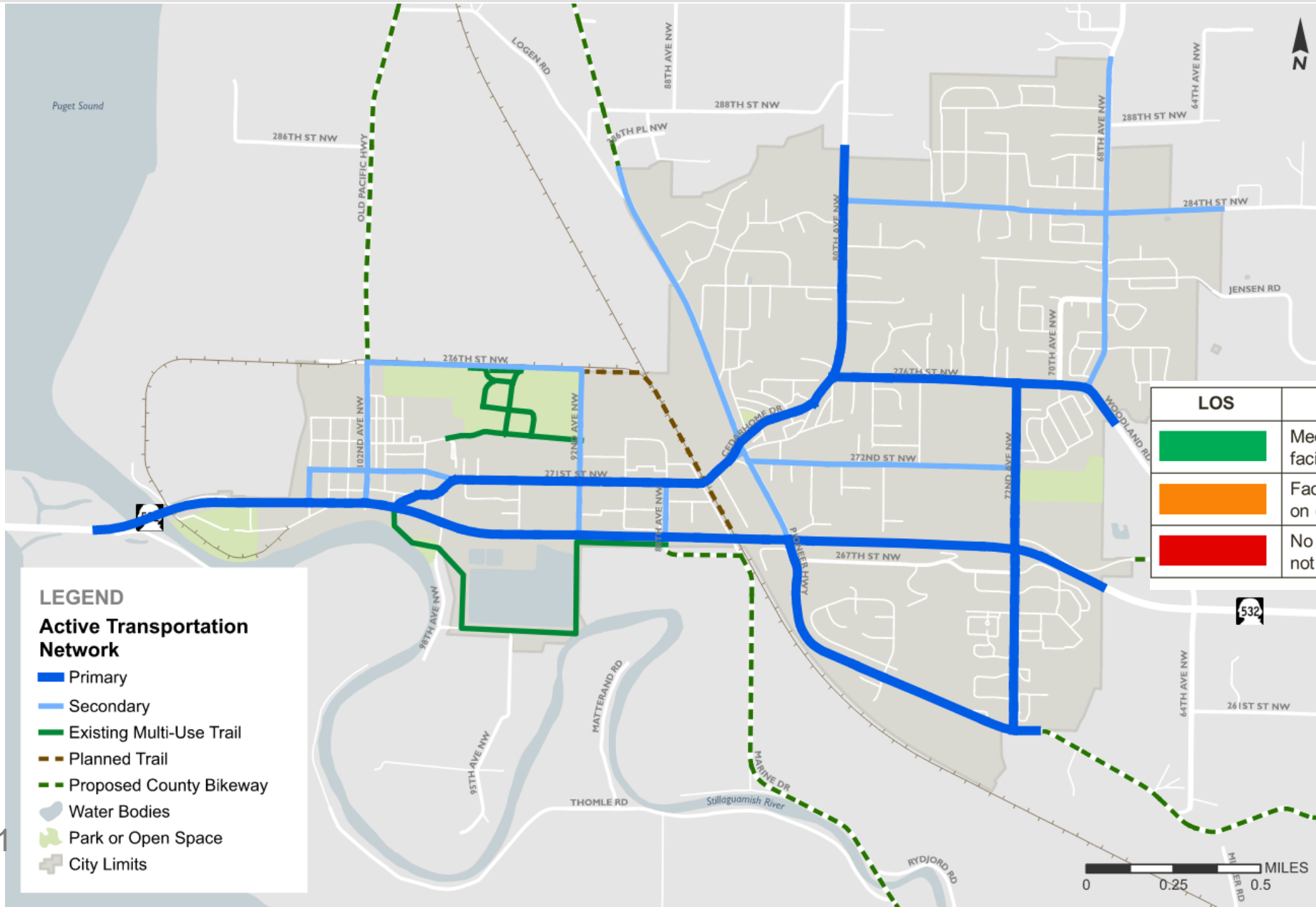
- Pedestrian / Bicycle LOS
  - Based on City's Design Standard along identified Active Transportation Network

**MINIMUM STREET DESIGN STANDARDS**

Design Standard	Arterial	Commercial/Neighborhood Major Collector	Neighborhood Minor Collector	Local Access/Cul-De-Sac
Transportation Standard Detail #	T-2	T-3	T-4	T-5/T-6
ROW Width (minimum)	86-98 ft.	57 ft.	49 ft.	49 ft.
Street Width (minimum)	48-60 ft. plus two 5 ft. bike lanes	36 ft.	28 ft.	28 ft.
Curb Requirements	Cement concrete curb (6") & gutter, both sides	Cement concrete curb (6") & gutter, both sides	Cement concrete curb (6") & gutter, both sides	Cement concrete curb (6") & gutter, both sides
Planter Width (minimum)	8.5 ft. wide both sides	5 ft. wide both sides	5 ft. wide both sides	5 ft. wide both sides
Sidewalk Requirements (minimum)	5 ft. wide both sides	5 ft. wide both sides	5 ft. wide both sides	5 ft. wide both sides
Utility Easements	10 ft. both sides	10 ft. both sides	10 ft. both sides	10 ft. both sides
Minimum-Maximum Grade	.50% to 7%	.50% to 12%	.50% to 12%	.50% to 14%

- E. Class I, II, III, or IV Bikeways, as appropriate, shall be provided when traffic analysis or traffic planning indicates substantial bicycle usage which would benefit from a designated bicycle facility as determined by the City except where noted herein.

# PLANNED ACTIVE TRANSPORTATION NETWORK



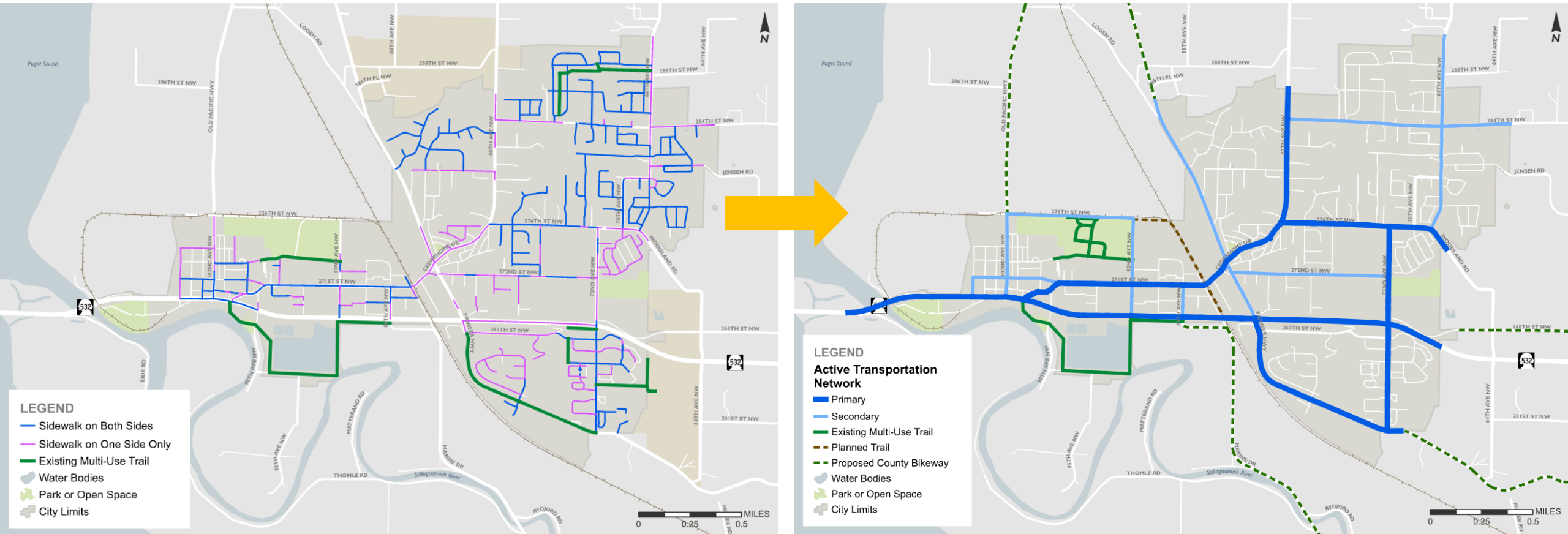
LOS	Primary Route	Secondary Route
	Meets City standards, facilities on both sides	Meets City standards, facilities on one or both sides
	Facilities exist, but only on one side	N/A
	No facilities exist, does not meet standards	No facilities exist, does not meet standards

**LEGEND**  
**Active Transportation Network**

- Primary
- Secondary
- Existing Multi-Use Trail
- Planned Trail
- Proposed County Bikeway
- Water Bodies
- Park or Open Space
- City Limits



# ACTIVE TRANSPORTATION NETWORK GAPS

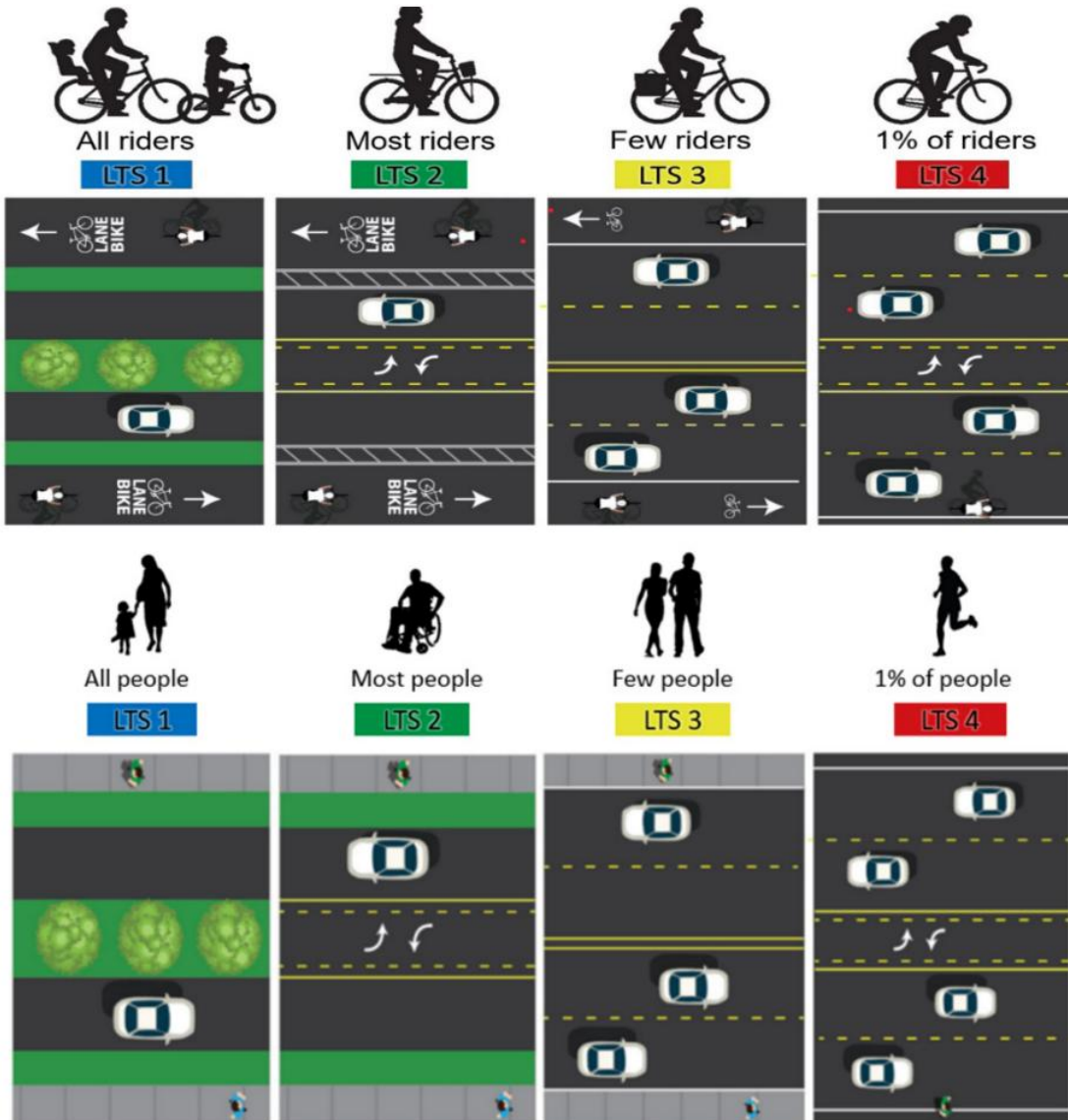


- Gaps in network become LOS ‘failures’ which result in capital projects.
  - These new capital projects can be included as part of updated TIFs

MMLOS OPTION 2 –

LEVEL OF TRAFFIC  
STRESS

# LEVEL OF TRAFFIC STRESS



Level of Traffic Stress	Description
1	Suitable for all ages and abilities; children could walk or bike here independently. Separated and/or barrier-protected.
2	Comfortable for most adults, including most adults experiencing disabilities. Some separation, no barrier.
3	Tolerable for enthusiastic and/or confident adults. Little space, no separation.
4	Only used by highly confident people, or those with no alternative. No dedicated space, no separation.

- WSDOT uses LTS 2 as standard for active transportation facilities on state roadways



# PEDESTRIAN LEVEL OF TRAFFIC STRESS



- LTS 3 and 4 would be considered 'failing'
- LTS can be improved by lowering speeds or adding facilities for pedestrians (sidewalks, multiuse paths, etc.)

# COMPARISON OF ALTERNATIVES

## 1. Design Based Method

- Pros: Simple, easy to administer, results in fewer LOS failures
- Cons: no current requirement for bicycle accommodation, if city updates design standard – many roadways will ‘fail’

## 2. LTS Method

- Pros: better measure of user experience, compatible with WSDOT, based on industry best practice
- Cons: slightly more complicated, results in more roadway ‘failures’

- Any remaining questions?
- Any preference on MMLOS alternatives?

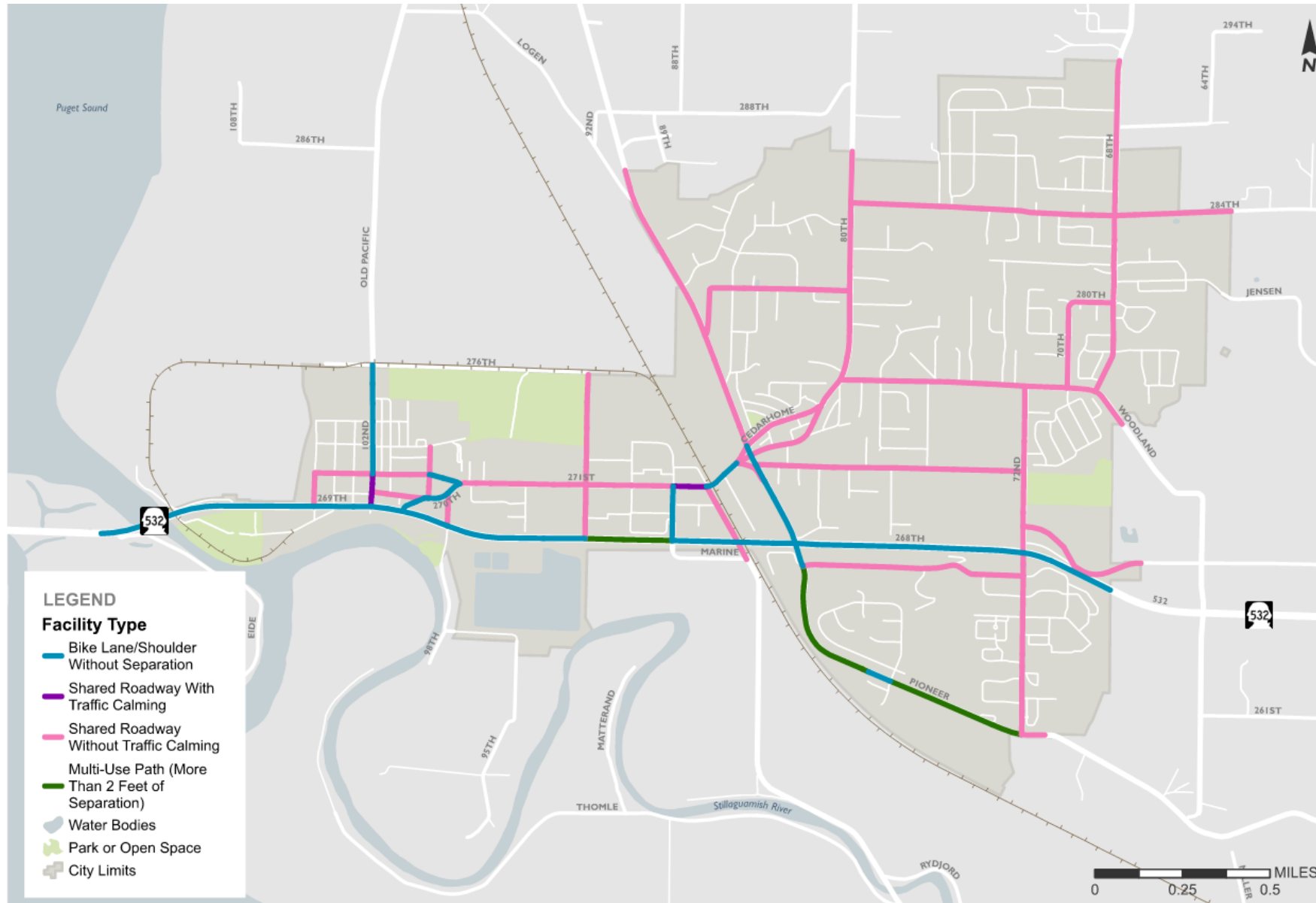
# NEXT STEPS

- Updated LOS methodology will be applied to Stanwood's transportation system
- New projects will be added to the long-range transportation project list
- Update transportation impact fees

# Q&A

# Backup slides

# BICYCLE FACILITIES





# BICYCLE LEVEL OF TRAFFIC STRESS

- BLTS range from 1 – 4
  - 1 = all ages and abilities
  - 4 = only the 'daring'

SHARED ROADWAY W/O TRAFFIC CALMING						
Lanes	ADT	Speed Limit				
		≤20	25	30	35	≥40
2 lanes	0-750	1	2	3	4	4
	750-1500	2	2	3	4	4
	1500-3000	2	2	3	4	4
	3000+	2	3	3	4	4
3 lanes	0-3,500	1	2	3	4	4
	3,500-7,000	2	2	3	4	4
	7,000-10,000	2	3	3	4	4
	10,000+	2	3	4	4	4

SHARED ROADWAY W/ TRAFFIC CALMING						
Lanes	ADT	Speed Limit				
		≤20	25	30	35	≥40
2 lanes	0-750	1	1	2	3	4
	750-1500	1	1	2	3	4
	1500-3000	1	2	2	3	4
	3000+	2	2	3	3	4
3 lanes	0-3,500	1	2	2	4	4
	3,500-7,000	2	2	3	4	4
	7,000-10,000	2	2	3	4	4
	10,000+	2	3	4	4	4

# PEDESTRIAN LEVEL OF TRAFFIC STRESS

NO PEDESTRIAN FACILITY						
Lanes	ADT	Speed Limit				
		≤20	25	30	35	≥40
2 lanes	0-750	1	1	3	4	4
	750-1500	1	2	3	4	4
	1500-3000	2	2	3	4	4
	3000+	3	3	4	4	4
3 lanes	0-3,500	2	2	3	4	4
	3,500-7,000	4	4	4	4	4
	7,000-10,000	4	4	4	4	4
	10,000+	4	4	4	4	4

SIDEWALK (5' TO 7.5' WIDE) WITHOUT BUFFER						
Lanes	ADT	Speed Limit				
		≤20	25	30	35	≥40
2 lanes	0-750	1	1	2	3	4
	750-1500	1	1	2	3	4
	1500-3000	1	2	2	4	4
	3000+	2	2	2	4	4
3 lanes	0-3,500	1	1	2	4	4
	3,500-7,000	2	2	2	4	4
	7,000-10,000	2	2	3	4	4
	10,000+	2	2	3	4	4

<sup>1</sup>"Without Buffer" indicates sidewalk is directly adjacent to the travel lane (i.e., no parking lane or bicycle lane)

SIDEWALK (8' WIDE OR MORE) AND/OR WITH BUFFER						
Lanes	ADT	Speed Limit				
		≤20	25	30	35	≥40
2 lanes	0-750	1	1	2	2	3
	750-1500	1	1	2	2	3
	1500-3000	1	1	2	2	3
	3000+	1	2	2	3	3
3 lanes	0-3,500	1	1	2	2	3
	3,500-7,000	1	2	2	3	4
	7,000-10,000	2	2	2	3	4
	10,000+	2	2	2	3	4

<sup>2</sup>"With Buffer" indicates that there is separation between the sidewalk and travel lane. This can be achieved through a striped shoulder, parking lane, or bicycle lane (minimum 3-foot width)

MULTI-USE PATH (MORE THAN 2 FEET OF SEPARATION)						
Lanes	ADT	Speed Limit				
		≤20	25	30	35	≥40
ANY	0-3,500	1	1	1	2	2
	3,500-7,000	1	1	1	2	2
	7,000-10,000	2	2	2	2	2
	10,000+	2	2	2	2	2